

# TOWN OF CANTON, CT ADA TRANSITION PLAN



**PIM**

**PRECISION  
INFRASTRUCTURE  
MANAGEMENT**

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## Introduction

The purpose of an American with Disabilities Act (ADA) Transition Plan is to ensure the residents and visitors of the Town of Canton have full access to the Town of Canton's public right-of-way and public facilities. It is designed to accommodate people with disabilities and give fair access to facilities without limiting their quality of life.

The Town of Canton is committed to meeting all the accessibility needs of disabled individuals. This ADA Transition Plan includes those plan components specifically requested by the Town of Canton to fulfill requirements of the ADA.

## Transition Plan and Purpose

The Americans with Disabilities Act is a civil rights law prohibiting discrimination against individuals on the basis of disability. It was enacted on July 26, 1990 and was amended in 2008 with the ADA Amendments Act. The ADA consists of five titles outlining protections in the following areas:

- I. Employment
- II. State and local government services
- III. Public accommodations
- IV. Telecommunications
- V. Miscellaneous provisions

As required by Title II of ADA (28 CFR Part 35 Sec. 35.105 and Sec. 35.150), the Town of Canton has conducted a self-evaluation of its public rights-of-way, and has developed this Transition Plan detailing the methods to be used to ensure compliance with ADA accessibility requirements.

## Agency and Requirements

Under Title II, the Town of Canton must meet these general requirements:

- Must designate at least one responsible employee to coordinate ADA compliance [28 CFR Sec. 35.107(a)]. This person is typically referred to as the ADA Coordinator. The public entity must provide the ADA Coordinator's name, office address, and telephone number to all interested individuals [28 CFR Sec. 35.107(a)].
- Must provide notice of ADA requirements. All public entities, regardless of size, must provide information about the rights and protections of Title II to applicants, participants, beneficiaries, employees, and other interested persons [28 CFR Sec. 35.106]. The notice must include the identification of the employee serving as the ADA Coordinator and must provide this information on an ongoing basis [28 CFR Sec. 104.8(a)].
- Must establish a grievance procedure. Public entities must adopt and publish grievance procedures providing for prompt and equitable resolution of complaints [28 CFR Sec. 35.107(b)]. This requirement provides for a timely resolution of all problems or conflicts related to ADA compliance before they escalate to litigation and/or the federal complaint process.

## Designation of Responsibility

In accordance with 28 CFR 35.107(a), The Town of Canton has designated the following person to serve as ADA Title II Coordinator:

- Name: Neil Pade
- Title: ADA Coordinator
- Phone: 860-693-7856
- Address: Canton Town Hall, P.O. Box 168, Collinsville, CT 06022

In accordance with 28 CFR 35.150(d)(3), The Town of Canton has designated the following person to serve as ADA Transition Plan Implementation Coordinator, to monitor Canton's progress and manage review and updates of this document:

- Name: Neil Pade
- Title: ADA Coordinator
- Phone: 860-693-7856
- Address: Canton Town Hall, P.O. Box 168, Collinsville, CT 06022

## Self-Evaluation

### Overview

Under Title II of the ADA (28 CFR Sec. 35.105), public entities are required to perform a self evaluation of their facilities on public property and within public rights-of-way, in order to identify any obstacles or barriers to accessibility that need to be addressed. The general categories of items to be evaluated include: sidewalk mileage calculation, vertical height displacement locations, severely degraded sidewalks requiring demolition and replacement, absence of curb ramps or detectable warnings, demolition and replacement, narrow or missing sidewalk sections. Public entities are required to provide an opportunity for interested persons, including individuals with disabilities or organizations representing individuals with disabilities, to participate in the self-evaluation process by submitting comments [28 CFR Sec. 35.105(b)].

## Process and Findings

In August 2024, a consultant on behalf of the Town of Canton completed a self-evaluation of its facilities within public rights-of-way regarding accessibility. The following types of right-of-way deficiencies were reviewed for specific sidewalk areas within approximately 7 miles of sidewalk:

- Vertical height displacement locations;
- Absence of curb ramps;
- General curb ramp assessments;
- Missing or narrow sidewalks;
- Cross slope issues generally;
- Areas that may require demolition and replacement;
- Pedestrian Access Route (PAR) areas under 4-ft in width due to structural issues or vegetation.

These deficiencies were cataloged in a geodatabase and provided to the Town of Canton.

The methodology used to conduct the condition study followed the ADA Guidelines for ADA Transition Planning, Public Right of Way Accessibility Guidelines, and Self- Assessment Checklists for Public ROW and Facilities. The references to these are provided below:

- ADA Transition Plan Guidelines Title II Checklist: [Checklist](#)
- Public Right-of-Way Accessibility Guidelines: [PROWAG](#)
- ADA Accessibility Guidelines: [ADAAG](#)
- ADA Guide for Small Municipalities: [ADA Guide](#)

An important component of the self-evaluation process is the identification of obstacles or barriers to accessibility and the corresponding modifications that will be needed to remedy these items. The following sections provide a summary of improvements and obstacles that the Town of Canton plans to address as part of this Transition Plan.



The Town of Canton plans to assess all public access facilities and to identify barriers to accessibility for programs and services.

## Public Involvement

The Town of Canton recognizes that public participation is an important component in the development of this transition plan. Input from the community has been gathered and used to help define priority areas for improvements within the jurisdiction of the Town of Canton.

Public involvement for preparation of this document has consisted of the following activities:

- Announcement of the ADA assessment in public forums and social media channels.
- Adoption of a Grievance Procedure.
- A public survey disseminated through the Town of Canton's website and available at the Town of Canton Town Hall. This document will also be made available to the public on the Town of Canton's website.

## Public Notice of ADA Requirements and Grievance Procedures

Under the Americans with Disabilities Act, each agency is required to publish its responsibilities with regard to ADA compliance. If users of the Town of Canton's facilities and services believe the Town of Canton has not provided a reasonable accommodation, they have the right to file a grievance. In accordance with 28 CFR Sec. 35.107(b), the Town of Canton has developed a grievance procedure for the purpose of the prompt and equitable resolution of citizens' complaints or concerns.

## Plan Implementation of Corrective Actions

- Prioritize identified issues.
- Inform and educate the Town of Canton officials of findings.
- Develop a budget for addressing issues.
- Coordinate a schedule of needed modifications with proposed remediations.

## Public Accessibility to Plan

The ADA Transition Plan will be available for review on the Town of Canton's website and a printed copy is available for review by request at the Canton Town Hall, at:

Canton Town Hall  
P.O. Box 168  
4 Market Street Canton  
Canton, CT 06022


## Public Accessibility to Plan

This ADA Transition Plan is hereby adopted by the Town of Canton, effective February of 2025.

Signed:

Neil Pade  
ADA Title II Coordinator Date

Kevin Witkos  
First Selectman



# TOWN OF CANTON, CT **RIGHT-OF-WAY SELF-ASSESSMENT**





# INTRODUCTION

Overview, Process, & Summary



**PIM**

PRECISION  
INFRASTRUCTURE  
MANAGEMENT



## Introduction

The Town of Canton, CT, contracted with Precision Infrastructure Management (PIM CS LLC) in 2024 to complete an Americans with Disabilities Act (ADA) Self-Assessment of the Town's public right-of-way (ROW) assets. PIM CS LLC completed the Self-Assessment in **September 2024**. This report is a comprehensive review of the assessment and includes an asset management plan to support the Town's budgeting and work planning processes.

The Study found a total of **1,012 unique ADA barriers across 7 miles of Right of Way sidewalk**. A breakdown of the barriers by category is covered in the ADA Barrier Detail section of this report.

## Self-Assessment

### Overview

Under Title II of the ADA (28 CFR Sec. 35.105), public entities are required to perform a Self-Assessment of their facilities on public property and within public rights-of-way (PROW) in order to identify any obstacles or barriers to accessibility that need to be addressed. The general categories of items evaluated for the Town's ROW include:

- Sidewalk mileage calculation
- Vertical Height Displacement locations
- Absence of curb ramps
- Curb ramps assessments
- Demolition and replacement areas
- Driveway cross slope issues
- Sidewalk with width < 4 ft.
- Cross slope > 4% for more than 50 ft.
- Sidewalk gaps and footpaths
- Obstructions
- Ponding in the Pedestrian Access Route and street

### Process & Findings

Precision Infrastructure Management employed ADA field assessment technicians to physically traverse each mile of sidewalk in Canton. Technicians used 2 ft. smart levels and tape measures to identify ADA barriers within the ROW. All data is stored within ESRI's ArcGIS program with photographs, GPS coordinates, and other associated metadata. The methodology used to conduct the condition study followed the Public Right of Way Accessibility Guidelines (PROWAG). These guidelines were promulgated into final rules for adoption by the United States Access Board in 2023. While they are not yet enforceable standards, it is current ADA best practice to use PROWAG standards for assessments as the expectation is that the standards will be enforceable in the future. In addition, the standards either equal or exceed current enforceable ADA requirements.

**7**  
**MILES**  
of Pedestrian ROW

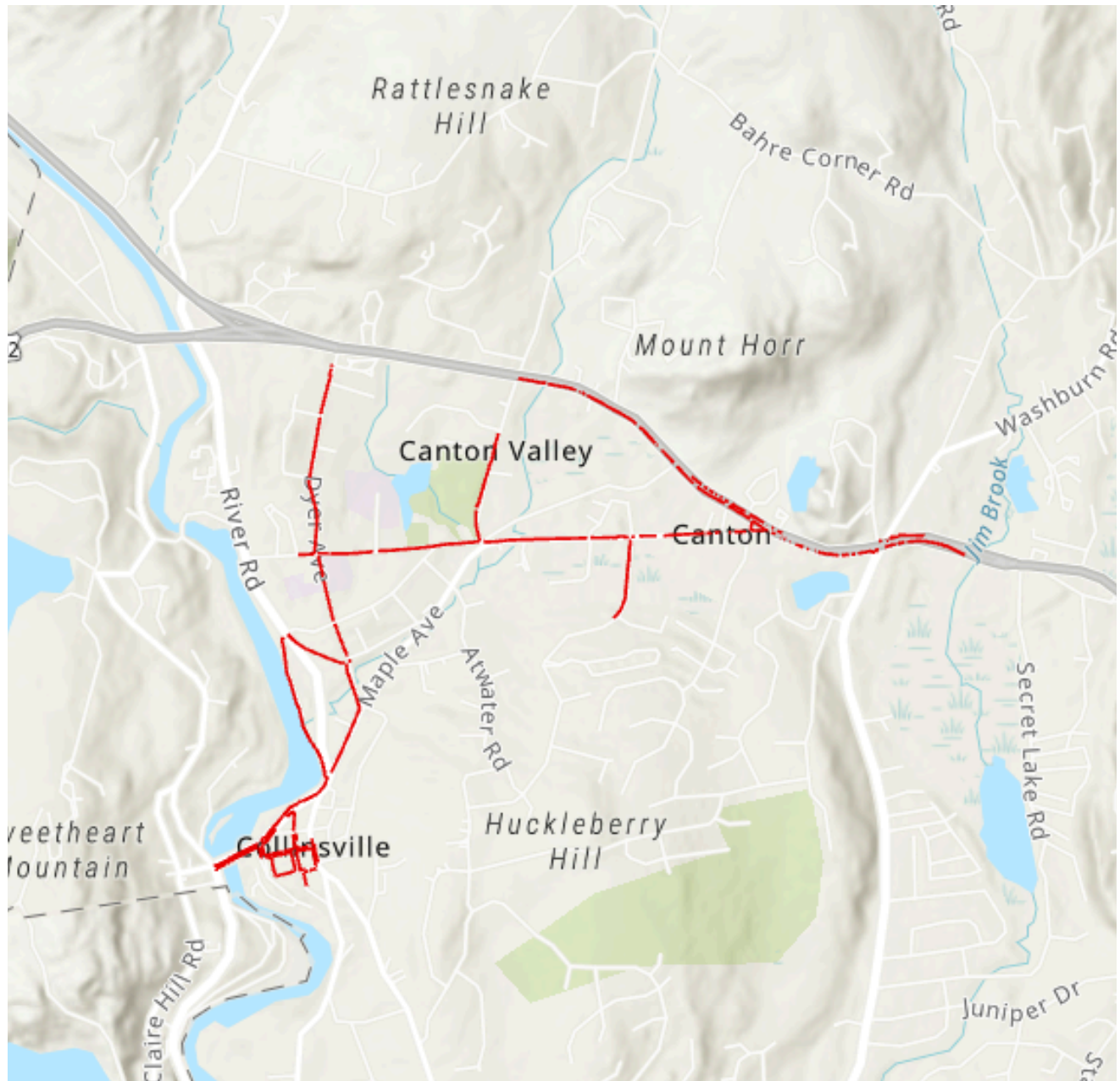


**1,012**  
**ADA BARRIERS**

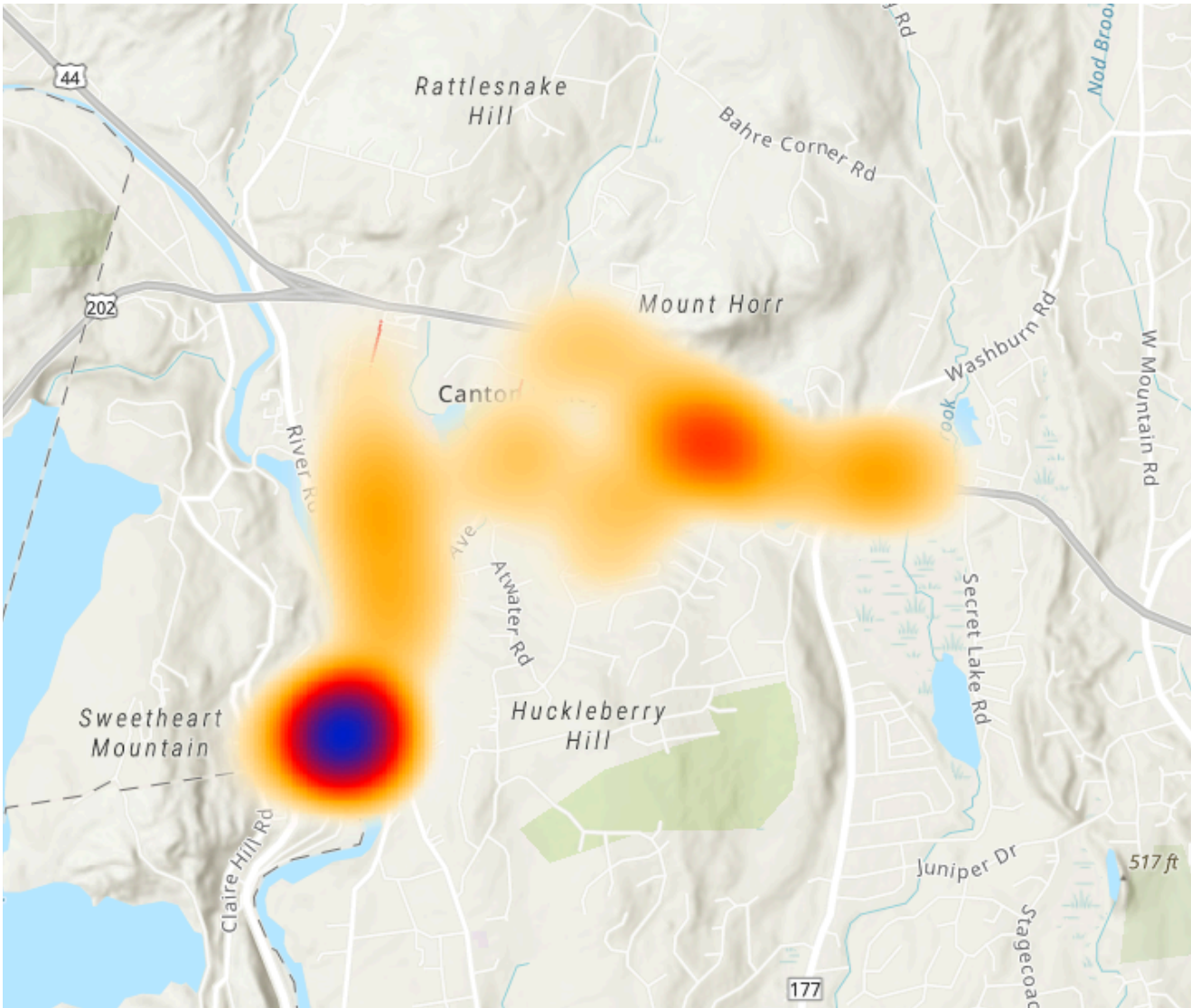


## Assessment Findings Summary

The field assessment of 7 miles of ROW sidewalk included in the Town of Canton sidewalk network identified a total of 1,012 ADA barriers across the assessment categories within the scope of the project. The deficiencies are reviewed in detail in the following sections. The map image below shows sidewalk locations across the Town.



**Caption:** Map of Canton, CT. Sidewalk locations are marked with a dark red line.



**Caption:** Map of Canton, CT. Heat map indicates ADA barriers identified in PIM's assessment.

## Heatmap Legend

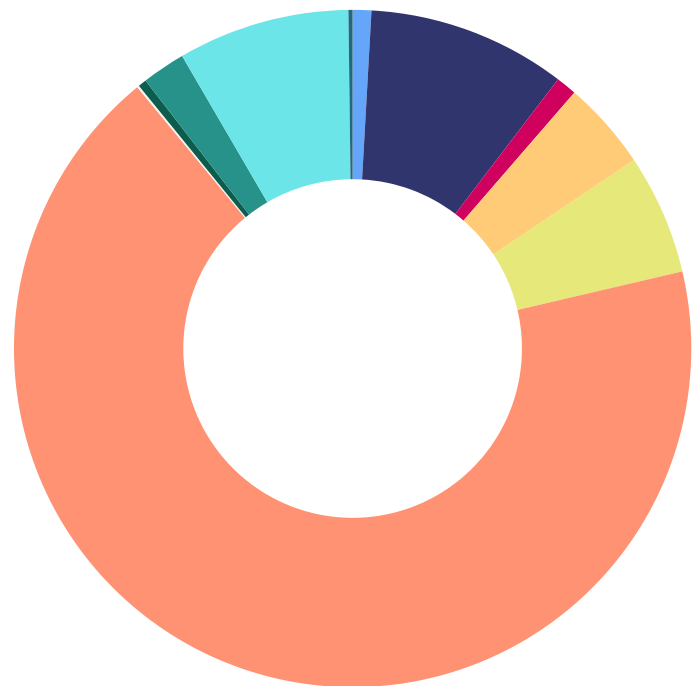
In the heatmaps shown in each barrier section, the legend shown here can be applied. Areas of no color have no barriers within them; Areas shown in red have a moderate number; Areas shown in blue have a higher concentration of barriers. The heatmap above shows the relative density of ADA barriers on different portions of Canton sidewalk.





## Total Findings - ADA Barriers

Cross Slope > 50 ft.	9
Crosswalks	21
Curb Ramps	96
Driveway Cross Slopes	43
No Curb Ramp	10
Obstructions	58
Vertical Height Displacements	685
Demolition & Replacement	83
Sidewalk Gaps and Footpaths	1
Sidewalk < 4 ft.	4
Ponding	2



**Vertical Height Displacements** make up **67.7%** of the total findings.

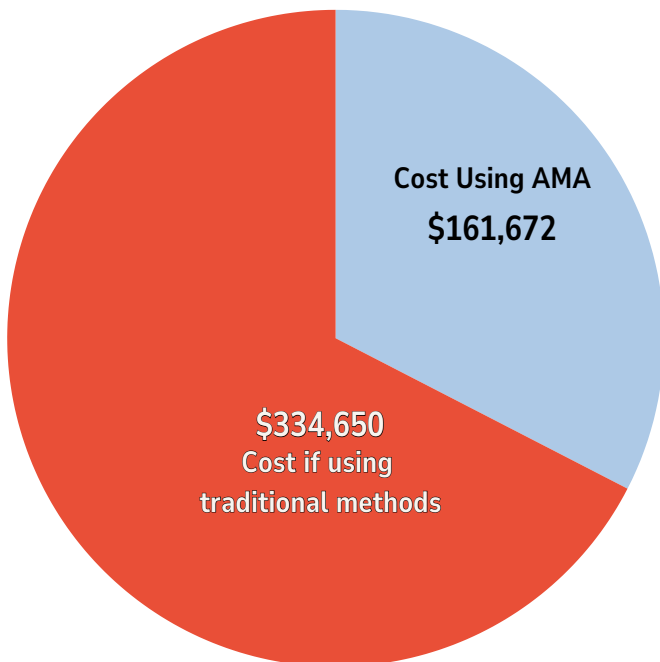
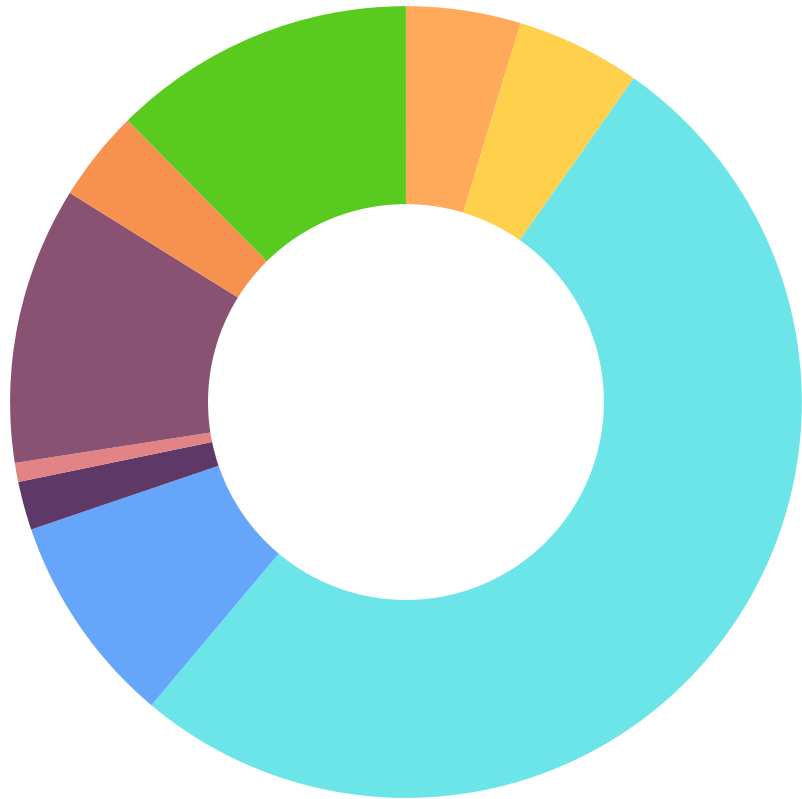
**Curb Ramps** make up **9.5%** of the total findings.

**Driveway Cross Slopes** make up **4.2%** of the total findings.

**The combined other deficiencies** consists of **18.6%** of the total findings.

## Estimated Cost by Barrier Class

Cross Slope	\$33,750
Crosswalks	\$36,750
Curb Ramps	\$371,000
Driveway Cross Slopes	\$62,625
Obstructions	\$14,300
Sidewalk Gaps	\$5,625
Vertical Height Displacements	\$81,750
Sidewalk < 4ft	\$27,000
Demo & Replacements	\$89,625



## Alternative Maintenance Activities vs. Total Cost Replacement

There are multiple methods for remediating certain ADA barriers. While some areas require full demolition and replacement of affected panels, other barriers can be mitigated using alternative maintenance activities (AMA). PIM collected data in such a manner to ensure that alternative maintenance activities could be utilized to remediate certain barriers, such as vertical height displacements (VHD) or, more rarely, curb ramps. **Utilizing alternative maintenance activities would save Canton more than \$172,000**

# STEPS IN SIDEWALK ASSET MANAGEMENT PLAN

01

Identify &  
Inventory

02

Inspect &  
Assess Condition

03

Analyze & Decide

04

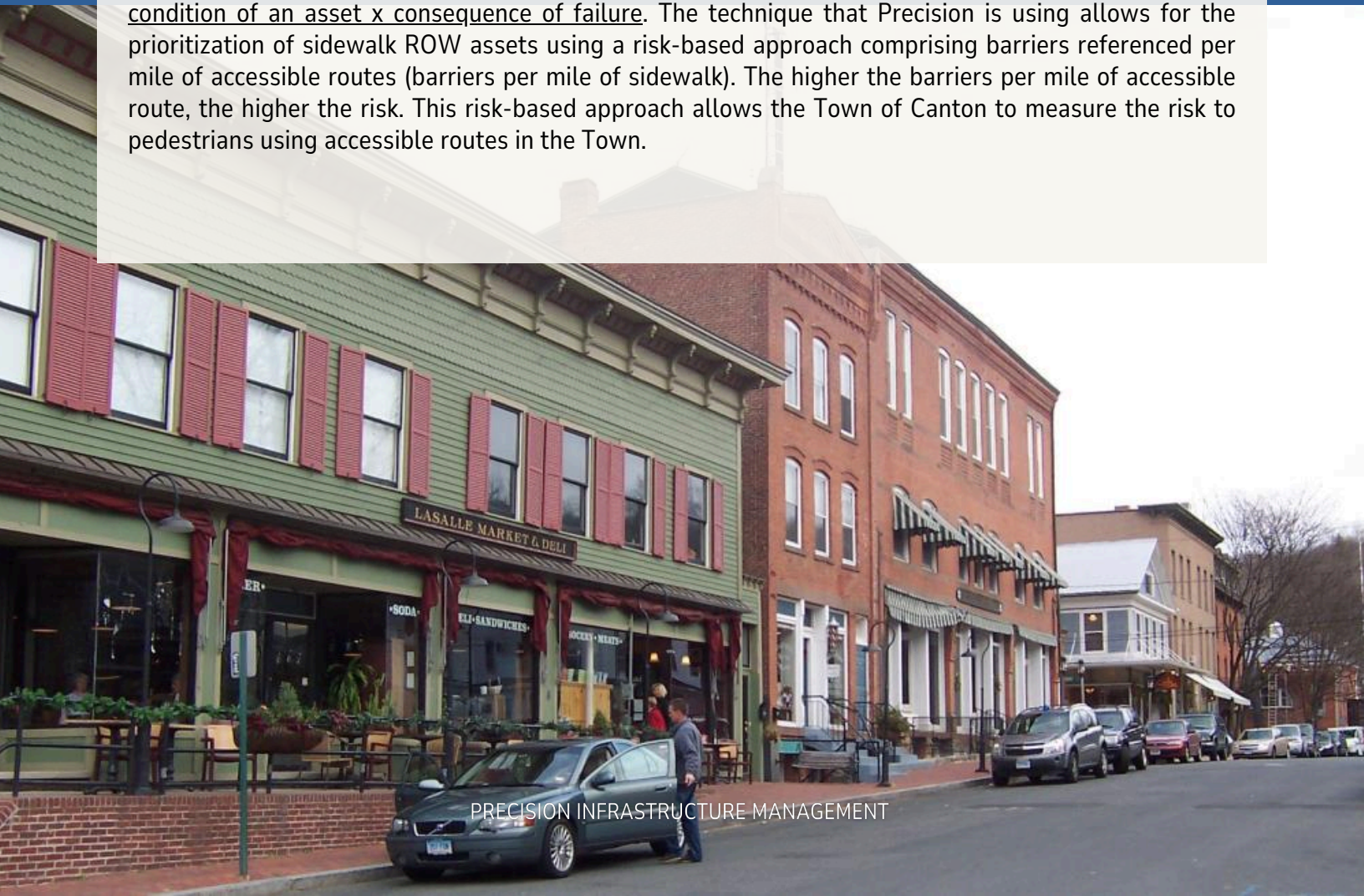
Prioritize Work

05

Repair or Demolish  
& Replace

## Prioritization Methodology

Risk, in the context of Canton's Sidewalk Asset Management Plan (SAMP), is defined as exposure to the chance of injury or loss. In asset management, risk is defined in a similar way, where risk = condition of an asset x consequence of failure. The technique that Precision is using allows for the prioritization of sidewalk ROW assets using a risk-based approach comprising barriers referenced per mile of accessible routes (barriers per mile of sidewalk). The higher the barriers per mile of accessible route, the higher the risk. This risk-based approach allows the Town of Canton to measure the risk to pedestrians using accessible routes in the Town.



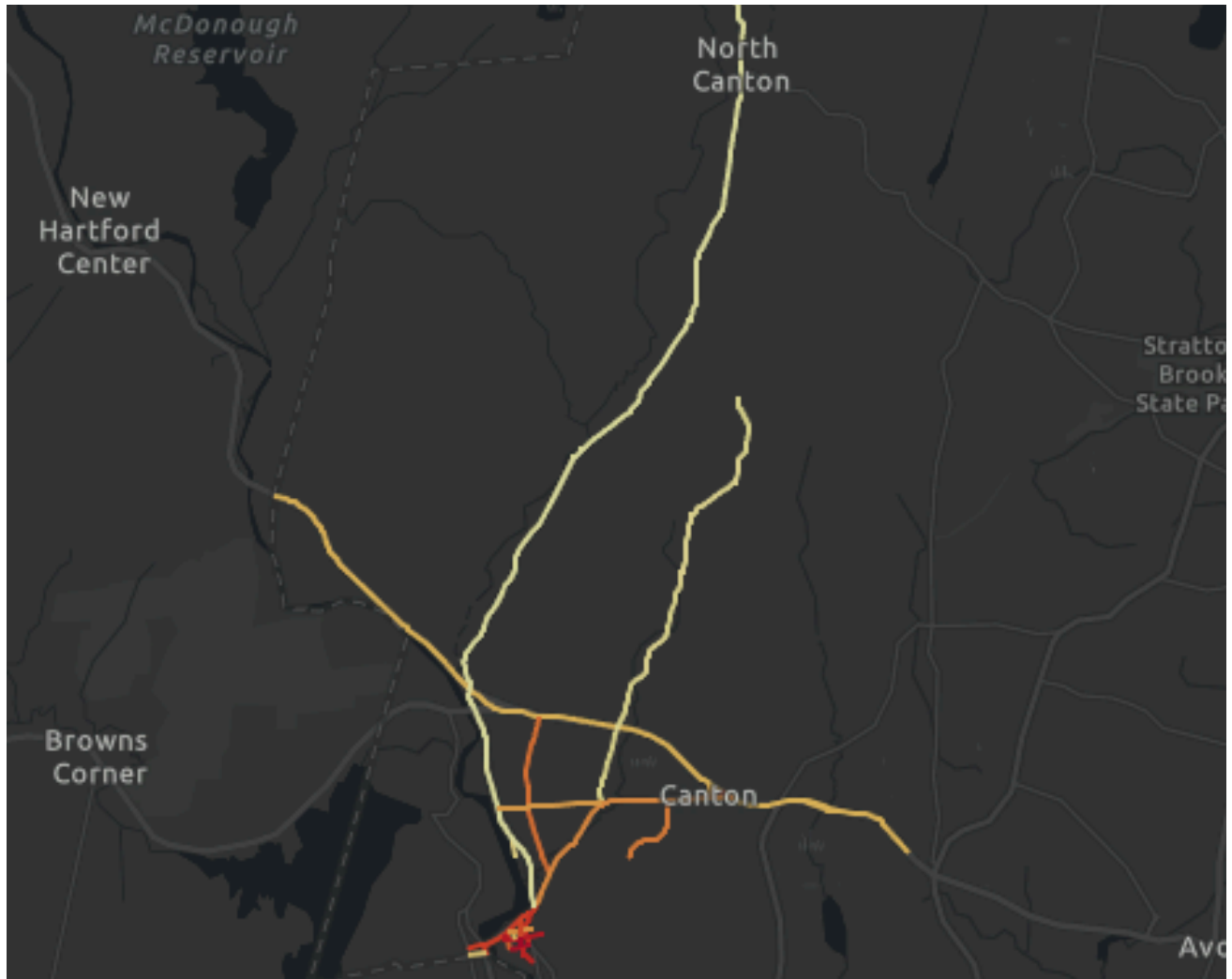


## Barriers and Prioritization

Self-Assessments surface a wealth of information about the condition of a Town's sidewalks, often leading to difficulty in deciding when/where/how to remediate barriers found during the assessment. To help prioritize areas that need remediation first, PIM sorted data by risk by street and by Census Block Groups.

These zones are used to collect United States Census and American Community Survey Data and allow cities to prioritize remediation based on selected demographic information.

For purposes of this assessment, risk is defined as the number of barriers per mile of sidewalk.



**Caption:** Map of Canton, CT. Barriers identified in PIM's assessment are color-coded by risk.

Above is a map of the Town of Canton's sidewalks, color-coded by risk. Red sidewalks have the highest risk factor, which is computed as ADA barriers per mile of sidewalk. Progressively lighter colors show streets with lower risk factors. A table of the top 10 streets with the highest barriers/mile is presented on the next page. The full list of streets is in an appendix at the end of this report.

<https://arcg.is/0jTT9C>

### Sidewalk Risk - Street Prioritization

Rank	Street Name	Barriers	Street Mileage	Barriers/Mile (Risk)
1	THE GREEN	27	0.07	411.29
2	SOUTH ST NO 1	27	0.14	194.58
3	MAIN ST	49	0.29	168.55
4	SPRING ST	21	0.14	145.17
5	CENTER ST	32	0.23	140.25
6	BRIDGE ST	79	0.62	127.19
7	RIVER ST	15	0.13	118.51
8	CANTON GREEN RD	5	0.05	110.13
9	FRONT ST	6	0.06	92.36
10	DYER AV	101	1.11	91.21

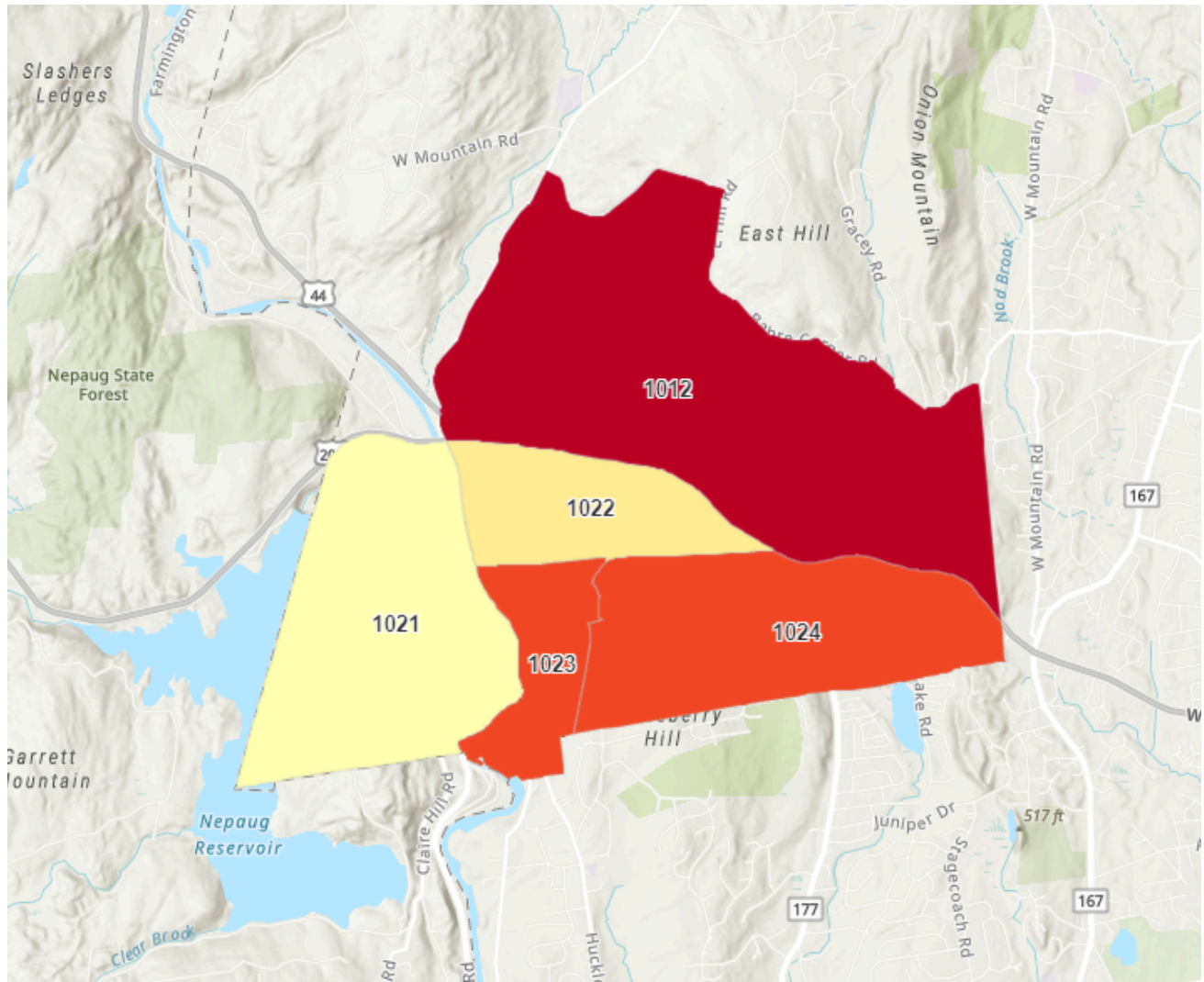


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For purposes of this assessment, risk is defined as the number of barriers per mile of sidewalk.



**Caption:** Map of Canton, CT. Barriers identified in PIM's assessment are color-coded by risk.

A map of the Town of Canton's Block Groups, color-coded by risk. Red Block Groups have the highest risk factor, which is computed as ADA barriers per mile of sidewalk. Progressively lighter colors show streets with lower risk factors. A table of the Block Group data is presented on the next page.

## Sidewalk Risk By Block Group

Block Groups	Barriers	Sidewalk Mileage	Barriers/ Sidewalk Mile (Risk)
1012	67	0.34	196.00
1024	112	0.71	158.82
1023	315	1.99	158.41
1022	234	2.95	79.40
1021	59	0.88	66.91

# CURB RAMP

## Assessment



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## Assessment Specification

- Absence of Ramp (where required)
- No Compliant Detectable Warning Device
- Running Slope too Great
- Cross Slope too Great
- Flare Slope too Great
- Counter Slope too Great
- Width Less Than 4 ft.
- No Flush Transition
- No Compliant Landing
- Ponding at Curb Ramp within Pedestrian Access Route (PAR)
- Vertical Height Displacement (VHD) on Curb Ramp

## Assessment Results

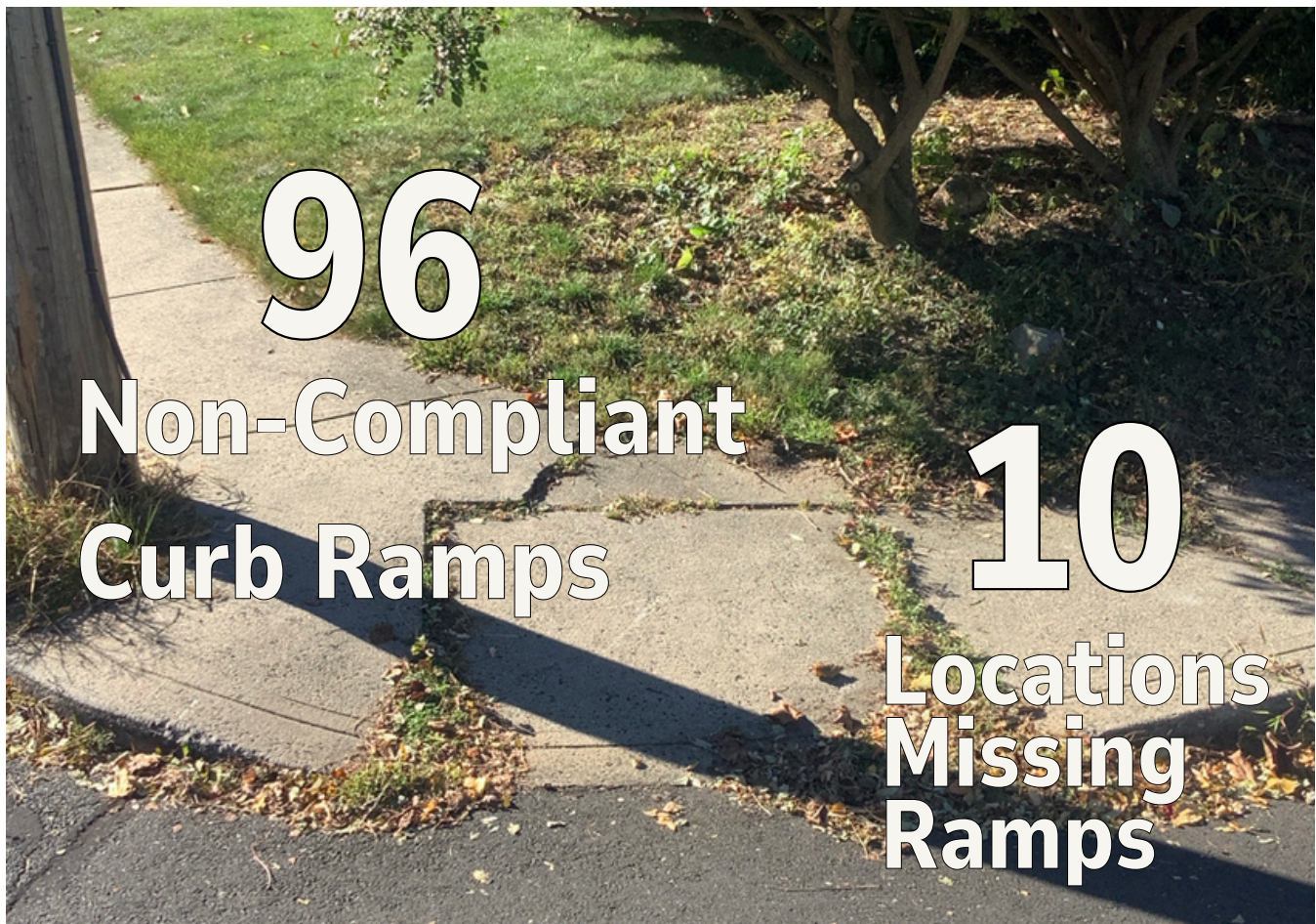
There were 117 curb ramps evaluated during the assessment. There were 10 locations requiring curb ramps that did not have them.

## Priority Repairs

Attribute	Qty
No Curb Ramp Locations	10
Non Compliant Curb Ramps	96
Compliant Curb Ramps According to Utilized Specifications	21
Total Assessed Curb Ramps	127

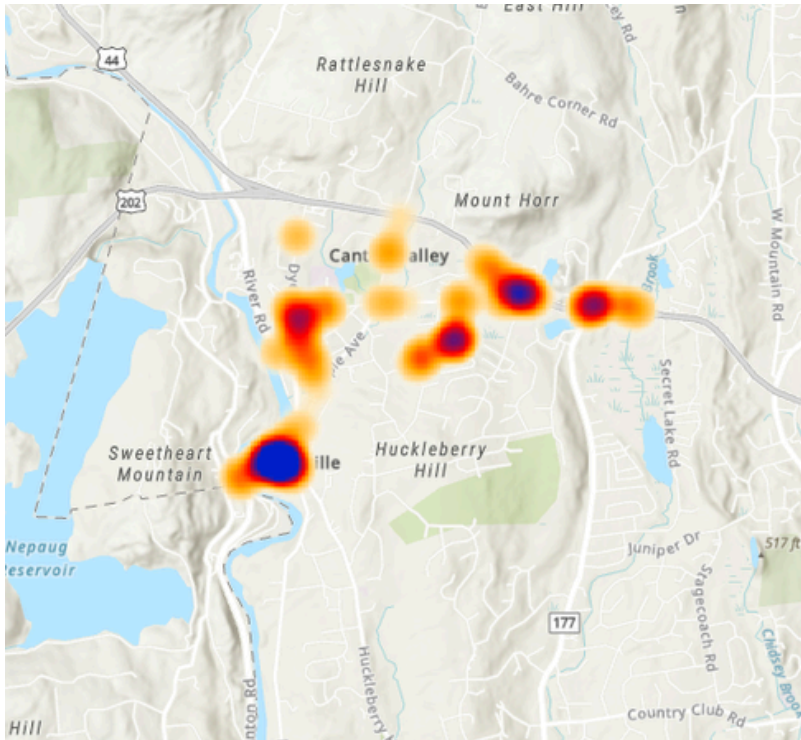
## Estimated Repair Costs

Curb Ramp Installation is projected to average \$3,500 per location. A prioritized summary of repair costs is provided in the report appendices.

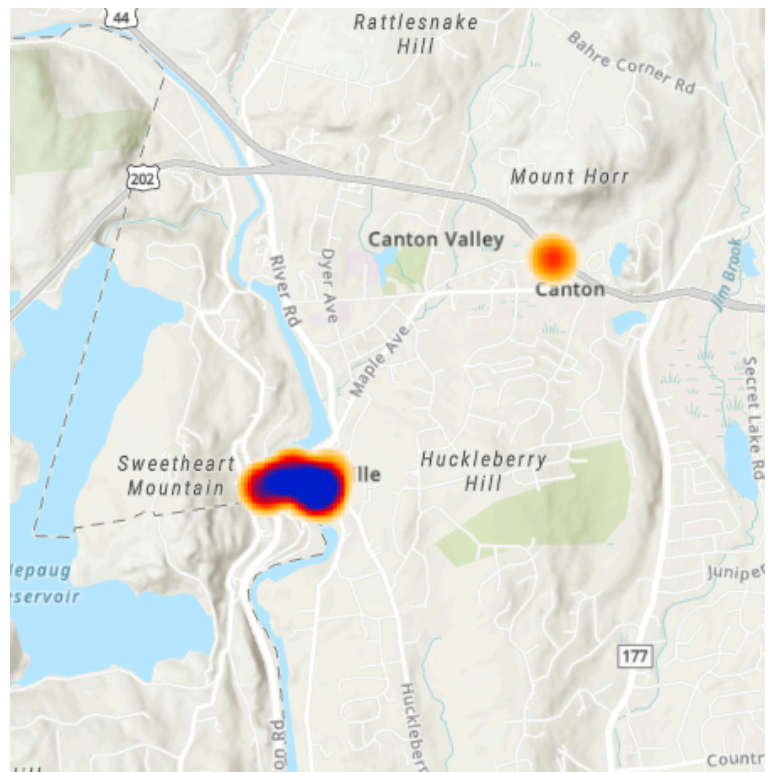




## Locations & Examples of Curb Ramp Barriers



## Locations & Examples with No Curb Ramp



## Repair Prioritization Methodology

Curb Ramps were prioritized for repair according to the severity of their deficiencies as well as -- for the most severe cases -- proximity to residential disability ramps, public facilities, and parks.

### Priority 1

Priority 1 includes curb ramps with any of the following barriers that are also within 1/16th mile of either a residential disability ramp, public facility, or park.

- No curb ramp
- Replacement required
- Inaccessible
- No compliant landing
- Width < 4 ft.
- Running slope > 12%
- Cross slope > 2.8%

### Priority 2

Priority 2 includes curb ramps with any of the following barriers that are not included in the P1 ranking.

- No curb ramp
- Replacement required
- Inaccessible
- No compliant landing
- Width < 4 ft.
- Running slope > 12%
- Cross slope > 2.8%

### Priority 3

Priority 3 includes curb ramps with any of the following barriers.

- Running slope between 8.3-12%
- Cross Slope between 2.1-2.8%

### Priority 4

Priority 4 includes curb ramps with any of the following barriers.

- Non-compliant detectable warning
- No flush transition
- Flare slope > 10%
- Counter slope > 5%
- Trip hazards on curb ramp

The following table shows the breakdown of each priority outlined above.

<b>Priority 1</b>	9
<b>Priority 2</b>	78
<b>Priority 3</b>	4
<b>Priority 4</b>	15

The web app button here shows locations of each of these curb ramps.

<https://arcg.is/1aWH010>



## Water Ponding in Accessible Routes

Water ponding at the bottom of curb ramps can create significant ADA barriers in the pedestrian access route, especially for those using wheelchairs. This accumulation of water can lead to slippery surfaces, increasing the risk of falls and injuries. For wheelchair users, water ponding presents a barrier that can make it difficult or even impossible to navigate the ramp. The wheels of a wheelchair can get stuck in the water, causing delays or forcing users to find alternative routes in the traffic lane, creating hazardous pedestrian experiences. Additionally, standing water can cause structural damage over time, leading to uneven surfaces that further impede accessibility. Such conditions are ADA barriers and pose safety hazards, reducing the overall usability of pedestrian pathways for everyone.

Ponding at the bottom of a curb ramp is a separate remediation issue from retrofitting a curb ramp, as installing a new ramp does not necessarily eliminate ponding issues.

### Assessment Results

2 locations had standing water or had signs of water ponding in Canton.

Ponding locations also create sediment build-up and degradation of the accessible route.

### Estimated Repair Costs

Remediation for ponding can cost between \$2,500 and \$15,000 depending on the severity of the ponding issue and whether alternative maintenance activity options are available.



**Caption:** Curb ramp with ponding.

## Locations & Examples of Ponding





# SIDEWALK CONDITIONS

Assessment



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## Sidewalk Conditions

### Assessment Specification

Sidewalk Vertical Height Displacement (VHD)  
Severity class:

- Small (.25 in. up to .49 in.)
- Medium (.5 in. up to .99 in.)
- Large (1.0 in. up to 2.5 in.)
- Demolish and Replace (>2.5 in. or > 4 cracks, significant spalling, unstable sections, large voids) [D&R]

### Sidewalk Conditions Results

There were 768 sidewalk conditions (VHD and D&R locations) recorded during the assessment.

## Vertical Height Displacement (VHD) & Demolition and Repairment (D&R) Totals

Small	399
Medium	229
Large	57
D & R	83

### Estimated Repair Costs

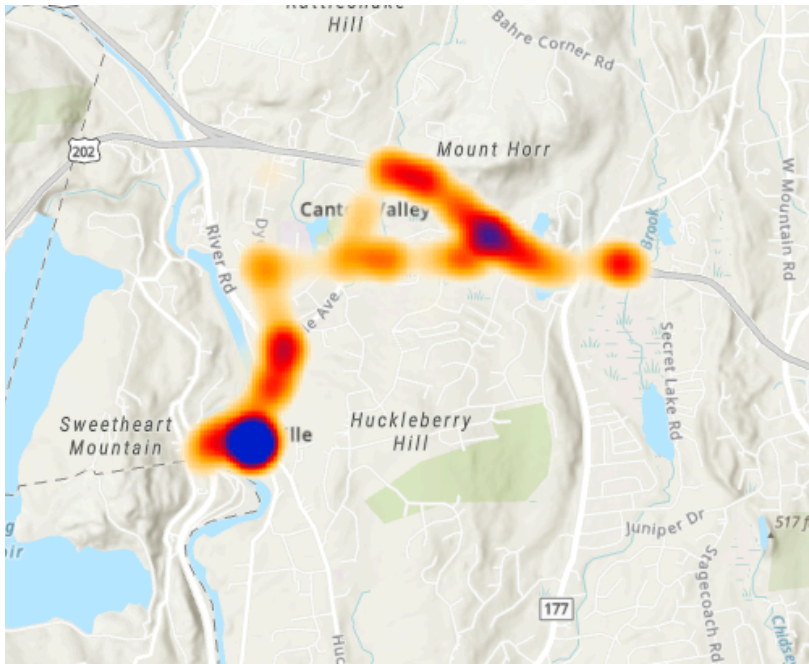
VHD repairs are based on an average cost of \$5 per square foot for remediation using an alternative maintenance activity like horizontal saw cutting.

D&R cost will be approximately \$15 per square foot of panel replaced.





## VHD Locations



SMALL (.25 in. up to .49 in.)



MEDIUM (.5 in. up to .99 in.)



LARGE (1.0 in. up to 2.5 in.)



DEMOLISH AND REPLACE (>2.5 in.)





# Obstructions

## Assessment Specification

The assessment looked at objects protruding within the ROW, of which a total of 58 were found during the assessment. The 58 obstructions are broken down into 3 fields shown below:

- Vegetative – Significant vegetation blocking the ROW.
- Ground – Physical barriers that obstruct the ROW. Examples include signposts, fire hydrants, and telephone poles.
- Vertical – Physical obstructions less than 80 inches off the ROW that create head clearance issues.

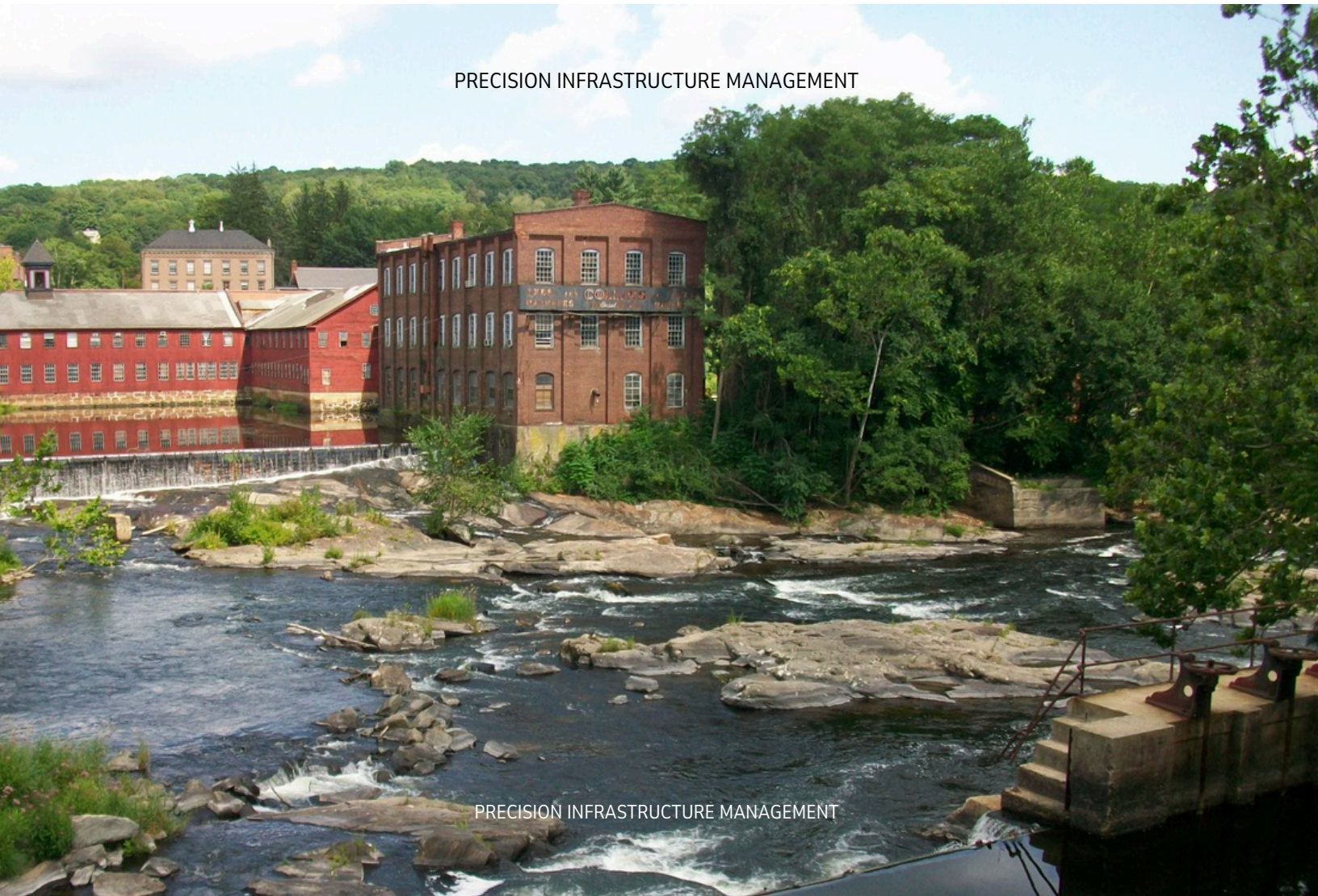
## Assessment Results

Vegetative	42
Ground	10
Vertical	6

## Estimated Repair Costs

Using data from previous projects, PIM estimates the cost of removing vegetative barriers at \$150 per obstruction and \$500 to mitigate ground/vertical obstructions. Costs for these items, especially ground obstructions, can vary widely depending on the nature of the obstruction.

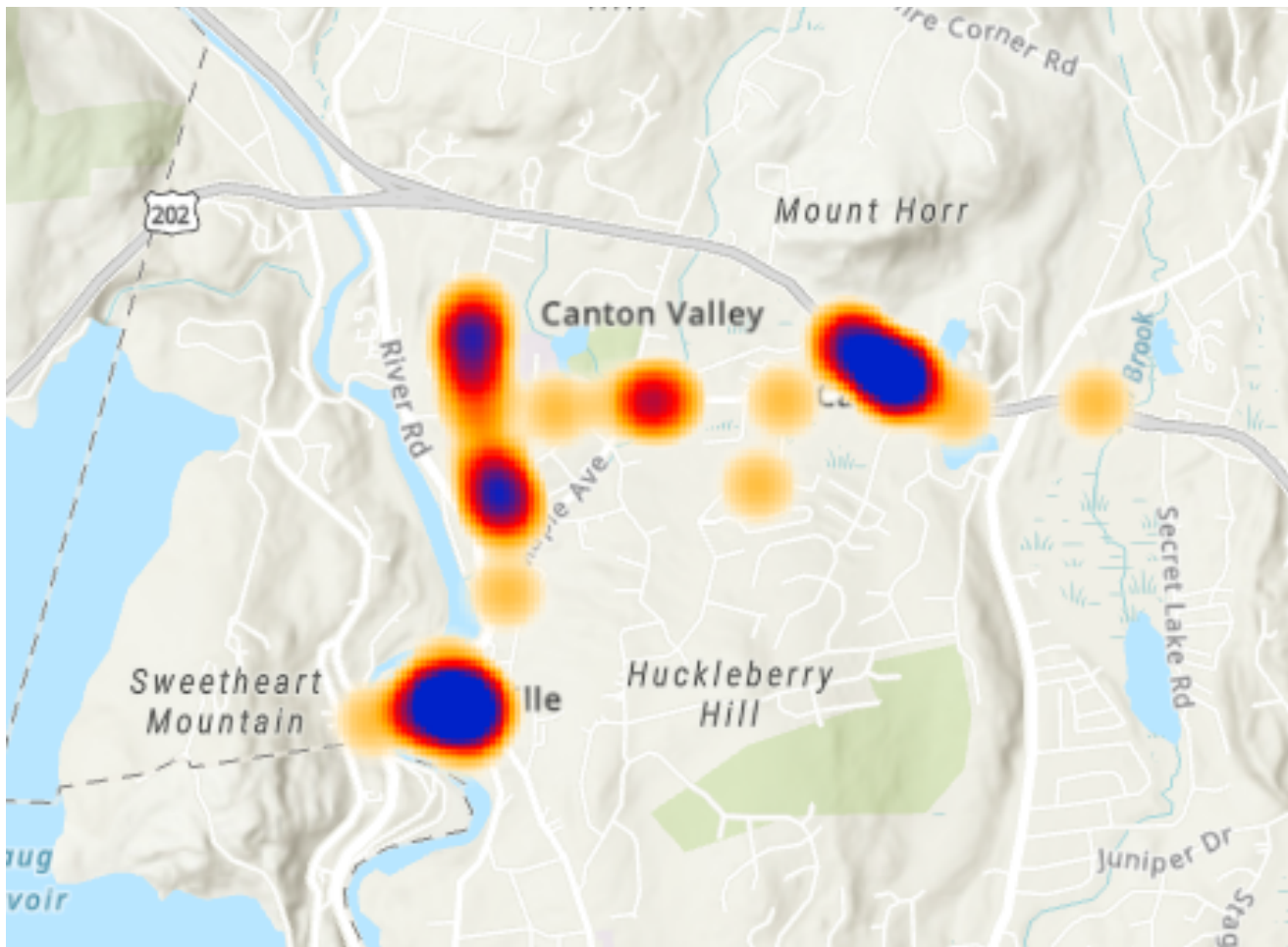
PRECISION INFRASTRUCTURE MANAGEMENT



PRECISION INFRASTRUCTURE MANAGEMENT



## Obstruction Locations and Examples





# Less Than 4 ft. Passable Surface

## Assessment Specification

PROWAG requires sidewalks to be 4 ft. wide or greater to provide adequate space for walking/wheeling.

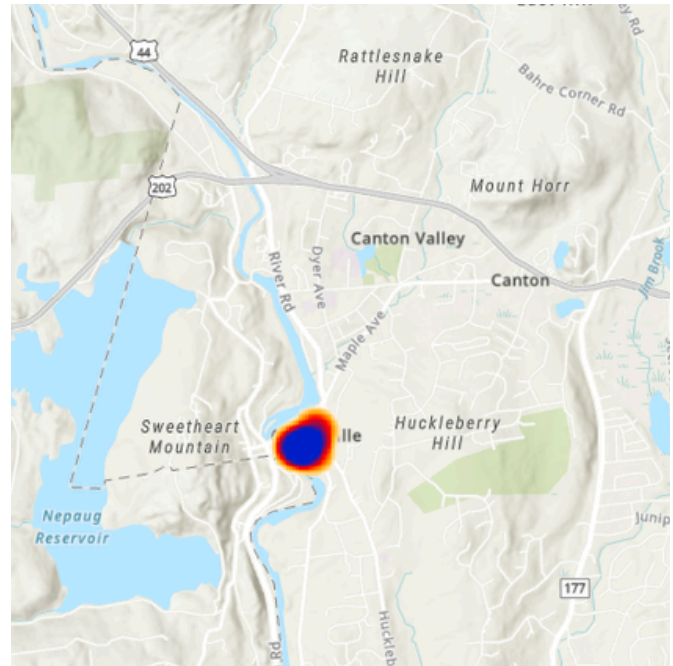
## Assessment Results

There were 4 sections of non-4 ft. wide passable sidewalk surfaces identified during the assessment due either to less than 4 ft. wide concrete pours or to erosion of the sidewalk resulting in a less than 4 ft. passable surface.

## Estimated Repair Costs

The only remedial action for sidewalk that is less than 4 ft. by design or by degradation is to demolish and replace the existing sidewalk. PIM estimates it would cost \$27,000 to replace the PROWAG non-compliant sections of sidewalk.

## Locations and Examples



## EXAMPLES





# Cross Slopes > 4% for more than 50 ft.

## Assessment Specification

PIM collected locations on Town sidewalk that had a greater than 4% cross slope for more than 50 ft. This collection item is intended to find the highest priority and most pervasive issues creating ADA barriers.

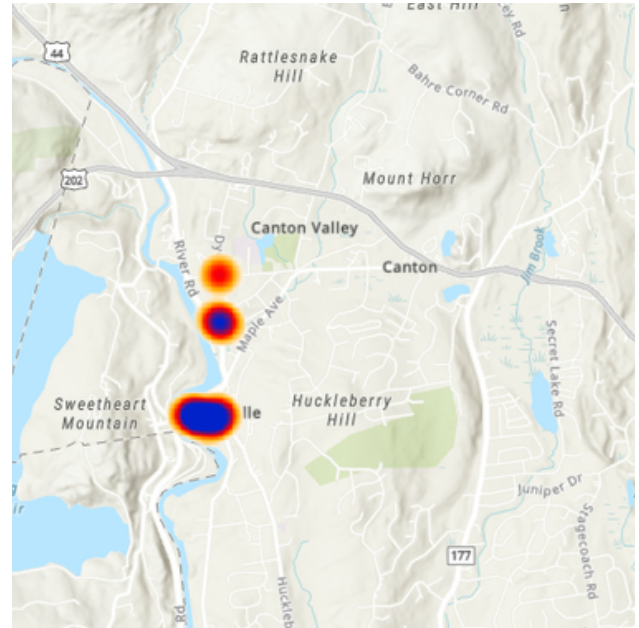
## Assessment Results

There were 9 such locations encountered during the assessment.

## Estimated Repair Costs

The only remedial action for extreme cross slope issues is to demolish and replace the existing sidewalk. PIM estimates the total cost of remediation for these cross slope issues is \$33,750.

## Locations and Examples



## EXAMPLES





# Driveway Cross Slopes

## Assessment Specification

For many decades driveways were designed in ways that are now considered non-compliant for the ROW. Typical design allowed homeowners or builders to cut through the existing sidewalk at slopes exceeding current cross slope requirements. PIM collected these barriers separately to allow Canton flexibility in remediation strategies.

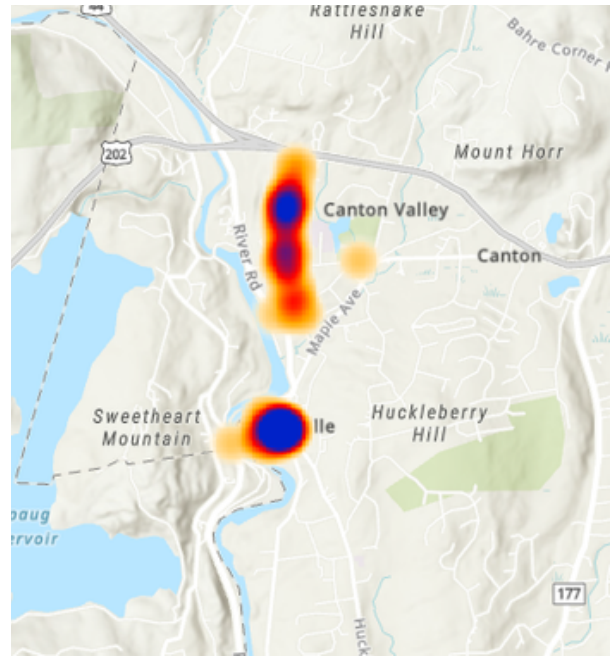
## Assessment Results

There were 43 such locations encountered during the assessment.

## Estimated Repair Costs

The only remedial action for driveway cross slope issues is to demolish and replace the existing sidewalk. PIM estimates the total cost of remediation for these cross slope issues is \$62,625.

## Locations and Examples



EXAMPLES





# CROSSWALKS and GAPS

Assessment



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# Crosswalks

## Assessment Specification

PIM assessed both ends of marked crosswalks within Town ROW to determine if their slope was compliant with the 5% running slope maximum allowable under PROWAG at uncontrolled crosswalks.

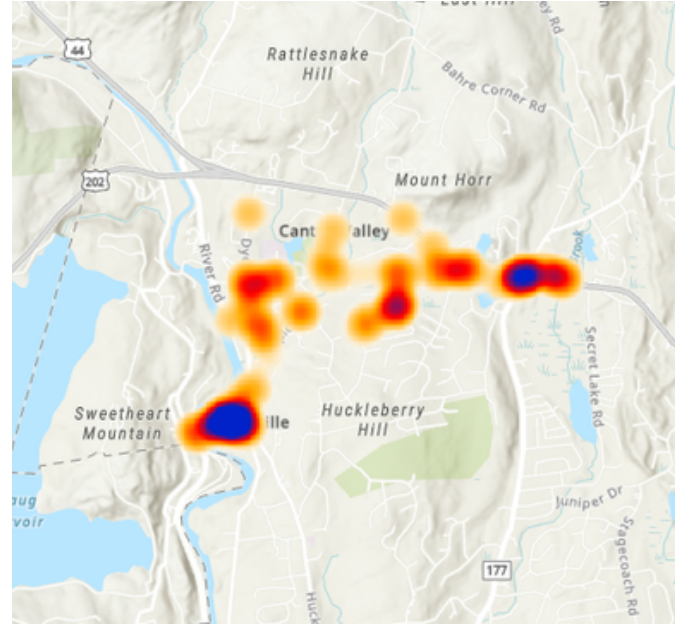
## Assessment Results

During the assessment, 21 crosswalks were found to have barriers.

## Estimated Repair Costs

The replacement/repair costs for these items vary widely due to the different nature of work required. PIM used \$1,750 as an estimate for cost per location. Total cost for repair is estimated to be \$36,750.

## Locations and Examples



EXAMPLES





# Gaps and Footpaths

## Assessment Specification

In addition to ADA compliance items, Canton requested that PIM flag sidewalk gaps and footpaths. These are areas where additional sidewalk would improve the pedestrian experience in a Town and generally increase accessibility for all residents and visitors.

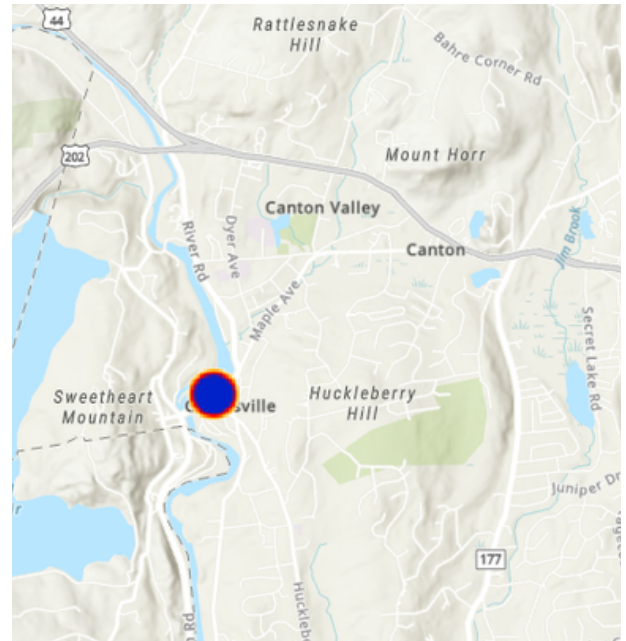
## Assessment Results

There was 1 such locations encountered during the assessment.

## Estimated Repair Costs

PIM estimates it would cost the Town of Canton \$5,625 to add the new sidewalk necessary to connect the sidewalk gaps and footpaths flagged during PIM's assessment.

## Locations and Examples



EXAMPLES





# Sidewalk Ends

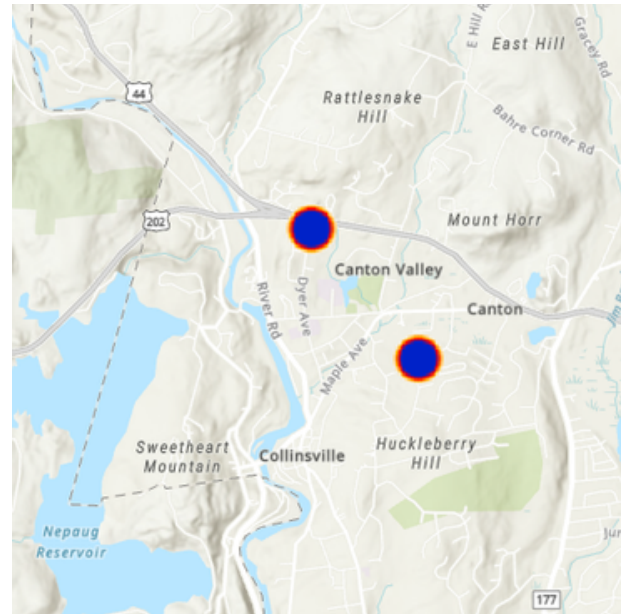
## Assessment Specification

In addition to ADA compliance items, Canton requested that PIM flag sidewalk that ends. These are areas where additional sidewalk could improve the pedestrian experience in a Town and generally increase accessibility for all residents and visitors.

## Assessment Results

There were 2 such locations encountered during the assessment.

## Locations and Examples



EXAMPLES





# ACCESSIBLE PEDESTRIAN SIGNALS

## APS Request Process Recommendation



## Accessible Pedestrian Signals (APS)

Accessible Pedestrian Signals (APS) play a crucial role in ensuring the greater accessibility of public rights of way, which include sidewalks, pedestrian crossings, and other pathways used by the public to navigate urban environments. The accessibility of these areas is vital for creating inclusive communities where individuals with disabilities can move about independently and safely.

### When APS May Be Required

Notwithstanding PROWAG and its anticipated adoption by the US Department of Justice and the requirements associated with pedestrian traffic control devices, deployment of accessible pedestrian signals may be required when a safety hazard has been identified or upon request by persons with disabilities.

## Requesting APS Installation

The following outlines the recommended procedures for requesting Accessible Pedestrian Signals.

1. Requests should be made through Canton's Public Works Department.
2. All APS installation requests should be reviewed and denied or approved by Public Works.
3. The Public Works Department should conduct a traffic study if the need and applicability are not obvious.
4. Public Works should document the process.
5. The specific type of APS should be determined on the needs identified in the study and input from relevant organizations.





# Glossary of Terms

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**Americans with Disabilities Act (ADA):** Civil rights law enacted in 1990 that prohibits discrimination against individuals with disabilities in all areas of public life, including employment, transportation, public accommodations, and government services.

**Alternative Maintenance Activities (AMA):** Cost-effective and less invasive methods for repairing or remediating ADA barriers, such as grinding down small vertical displacements instead of fully replacing sidewalk panels.

**Curb Ramp:** A ramp that allows individuals to transition from a sidewalk to the street, facilitating access for wheelchair users, people with strollers, and others. ADA-compliant curb ramps have specific slope, width, and surface requirements to ensure safety and usability.

**Cross Slope:** The slope of a sidewalk or other walking surface perpendicular to the direction of travel. For accessibility, the cross slope must not exceed a specific gradient, typically 2%, to ensure that it is safe for all users, including those who use mobility devices.

**Obstruction:** Any object that blocks or impedes passage within the public right of way. Examples include signposts, fire hydrants, or vegetation that encroaches on the sidewalk. These obstructions must be mitigated to ensure clear and accessible pedestrian routes.

**Ponding:** The accumulation of water on pedestrian routes, such as sidewalks or curb ramps, that creates a barrier to accessibility. Ponding can make it difficult or impossible for wheelchair users to navigate, and it can also cause slippery conditions that are hazardous to all pedestrians.

# Glossary of Terms

---

**PROWAG (Public Rights-of-Way Accessibility Guidelines):** Guidelines developed by the U.S. Access Board that provide design standards to ensure accessibility in public rights of way, such as sidewalks, crosswalks, and pedestrian signals. PROWAG aims to ensure that these areas are accessible to all individuals, including those with disabilities.

**Running Slope:** The slope of a walking surface in the direction of travel. For sidewalks and curb ramps, the running slope must meet specific ADA standards to be considered accessible, usually not exceeding a slope of 5% for pedestrian pathways.

**Self-Assessment:** A process required under Title II of the ADA, where public entities evaluate their facilities and public rights of way to identify and address barriers to accessibility. The self-assessment serves as a foundational step in developing transition plans and ensuring compliance with ADA requirements.

**Sidewalk Gap:** A missing section of sidewalk within a pedestrian route. Gaps can significantly hinder accessibility, especially for individuals with disabilities, by forcing them to find alternative, and potentially unsafe, routes.

**Vertical Height Displacement (VHD):** A condition where the vertical height difference between adjacent sidewalk panels exceeds the allowable limit, creating a barrier to accessibility. Typically, a VHD of 0.25 inches or greater is considered an obstacle that requires remediation under ADA standards.



# APPENDIX A

## Sidewalk Asset Management Plan



**PIM** | PRECISION  
INFRASTRUCTURE  
MANAGEMENT



# FIRST YEAR COSTS

CANTON, CT ADA SELF ASSESSMENT

## CURB RAMPS

Cost (\$) Locations

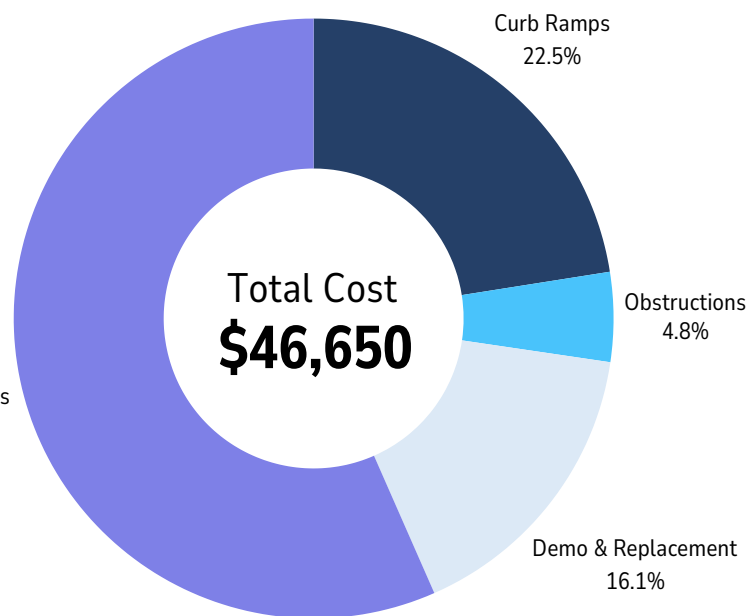
Priority 1 - Curb Ramps	\$10,500	3
Priorities 2-4 Curb Ramps	\$0	0

## OBSTRUCTIONS

Vegetative	\$2,250	15
Ground	\$0	0
Vertical	\$0	0

## SIDEWALK CONDITIONS

Demolish & Replace	\$7,500	20
Vertical Height Displacements	\$26,400	211
Sidewalk < 4 ft.	\$0	0
Sidewalk Gaps	\$0	0
Cross Slope	\$0	0
Driveway Cross Slope	\$0	0



## 1st Year Asset Management Plan

**Total Cost estimated** for the first year over all categories is **\$46,650**.

**33% of P1 Curb Ramp Repair** is completed during the 1st year. The total cost for Curb Ramp repair is **\$10,500 for 3 locations**.

**36% of Obstructions** are removed during the 1st year. The total cost is **\$2,250 for 15 locations**.

**32% of VHD Repair** is completed during the 1st year. The total cost for Curb Ramp repair is **\$26,400 for 211 locations**.

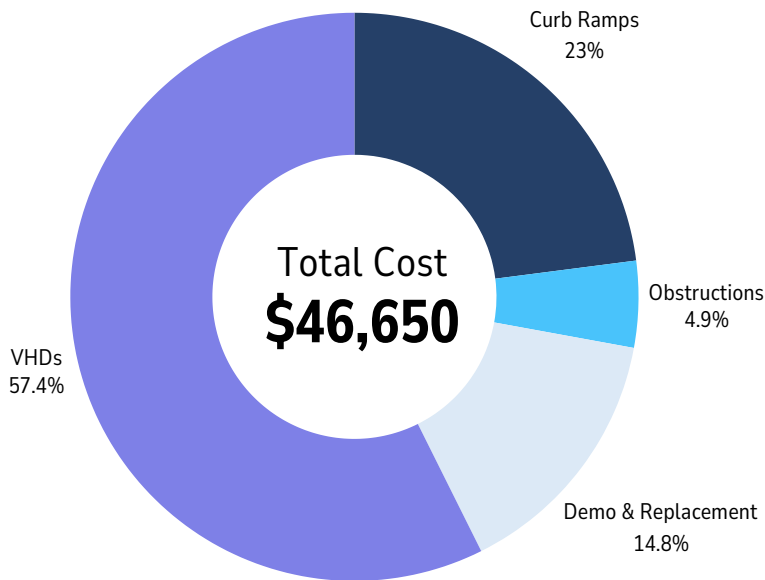
**8% of DNR** is completed during the 1st year. The total cost for Curb Ramp repair is **\$7,500 for 20 locations**.

**17.5%**  
OF ENTIRE PLAN

Canton's Est. Budget will cover 17.5% of remediation in the first year.



# SECOND YEAR COSTS



## CURB RAMPS

	Cost (\$)	Locations
P1 - Curb Ramps	\$10,710	3
Priorities 2-4 Curb Ramps	\$0	0

## OBSTRUCTIONS

Vegetative	\$2,295	15
Ground	\$0	0
Vertical	\$0	0

## SIDEWALK CONDITIONS

Demolish & Replace	\$6,885	18
Vertical Height Displacements	\$26,760	210
Sidewalk < 4 ft.	\$0	0
Sidewalk Gaps	\$0	0
Cross Slope	\$0	0
Driveway Cross Slope	\$0	0

## 2nd Year Management Plan

**Total Cost estimated** for the 2nd year over all categories is **\$46,650**.

**33% of P1 - Curb Ramp Repair** is completed during the 2nd year. The total cost for Curb Ramp repair is **\$10,710 for 3 locations**.

**36% of Vegetative Obstructions** are removed. The total cost is **\$2,295 for 15 locations**.

**32% of VHD** are removed. The total cost is **\$26,760 for 210 locations**.

**8% of DNR** are removed. The total cost is **\$6,885 for 18 locations**.

**17.3%**  
OF ENTIRE PLAN

Canton's Est. Budget will cover 17.3% of remediation in the 2nd year.

# THIRD YEAR COSTS

CANTON, CT ADA SELF ASSESSMENT

## CURB RAMPS

Cost (\$) Locations

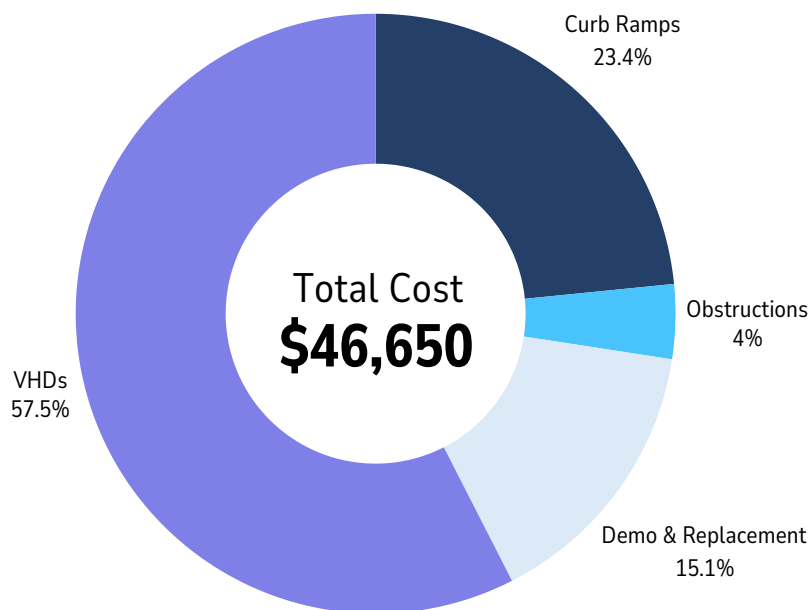
Priority 1 - Curb Ramps	\$10,924	3
Priorities 2-4 Curb Ramps	\$0	0

## OBSTRUCTIONS

Vegetative	\$1,873	12
Ground	0	0
Vertical	0	0

## SIDEWALK CONDITIONS

Demolish & Replace	\$7,023	18
Vertical Height Displacements	\$26,830	206
Sidewalk < 4 ft.	\$0	0
Sidewalk Gaps	\$0	0
Cross Slope	\$0	0
Driveway Cross Slope	\$0	0



## 3rd Year Asset Management Plan

**Total Cost estimated** for the third year over all categories is **\$46,650**.

**33% of P1 - Curb Ramp Repair** is completed during the 3rd year. The total cost for Curb Ramp repair is **\$10,924 for 3 locations**.

**31% of VHD** is also completed by the 3rd year. The total cost for Sidewalk D&R is **\$26,830 for 206 locations**.

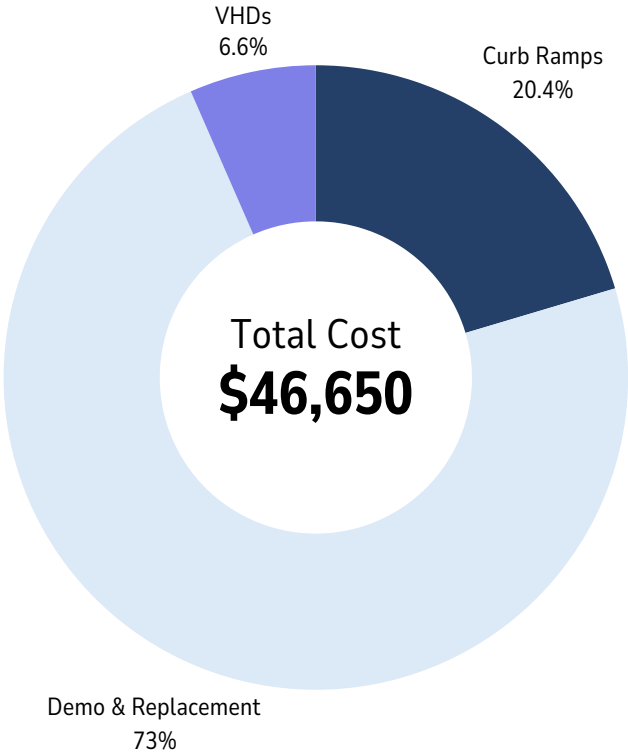
**8% of DNR** is also completed by the 3rd year. The total cost for Sidewalk D&R is **\$7,023 for 18 locations**.

**16.8%**  
OF ENTIRE PLAN

Canton's Est. Budget will cover 16.8% of remediation in the third year.



# FOURTH YEAR COSTS



## 4th Year Management Plan

**Total Cost estimated** for the 4th year over all categories is **\$46,650**.

**4% of P2 - Curb Ramp Repair** is completed during the 4th year. The total cost for Curb Ramp repair is **\$11,145 for 3 locations**.

**4% of VHDs** are removed. The total cost is **\$3,585 for 27 locations**.

**33% of DNR** is completed during the 4th year. The total cost is **\$39,920 for 80 locations**.

### CURB RAMPS

	Cost (\$)	Locations
P1 - Curb Ramps	\$0	0
Priorities 2-4 Curb Ramps	\$11,145	3

### OBSTRUCTIONS

Vegetative	\$0	0
Ground	\$0	0
Vertical	\$0	0

### SIDEWALK CONDITIONS

Demolish & Replace	\$39,920	80
Vertical Height Displacements	\$3,585	27
Sidewalk < 4 ft.	\$0	0
Sidewalk Gaps	\$0	0
Cross Slope	\$0	0
Driveway Cross Slope	\$0	0

**7.7%**  
**OF ENTIRE PLAN**

Canton's Est. Budget will cover 7.7% of remediation in the 4th year.

# FIFTH YEAR COSTS

CANTON, CT ADA SELF ASSESSMENT

## CURB RAMPS

Cost (\$) Locations

Priority 1 - Curb Ramps	\$0	0
Priorities 2-4 Curb Ramps	\$22,735	6

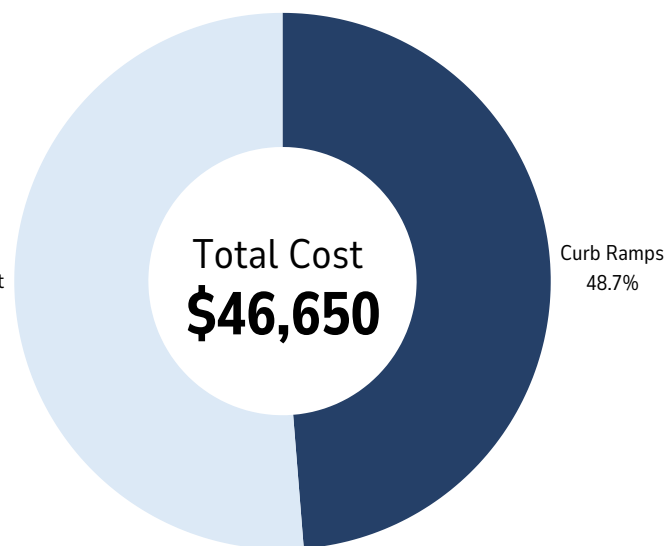
Demo & Replacement  
51.3%

## OBSTRUCTIONS

Vegetative	\$0	0
Ground	\$0	0
Vertical	\$0	0

## SIDEWALK CONDITIONS

Demolish & Replace	\$23,915	59
Vertical Height Displacements	\$0	0
Sidewalk < 4 ft.	\$0	0
Sidewalk Gaps	\$0	0
Cross Slope	\$0	0
Driveway Cross Slope	\$0	0



## 5th Year Asset Management Plan

**Total Cost estimated** for the fifth year over all categories is **\$46,650**.

**4% P2 - Curb Ramp** locations are remediated. The total cost for Curb Ramp repair is **\$22,735 for 6 locations**.

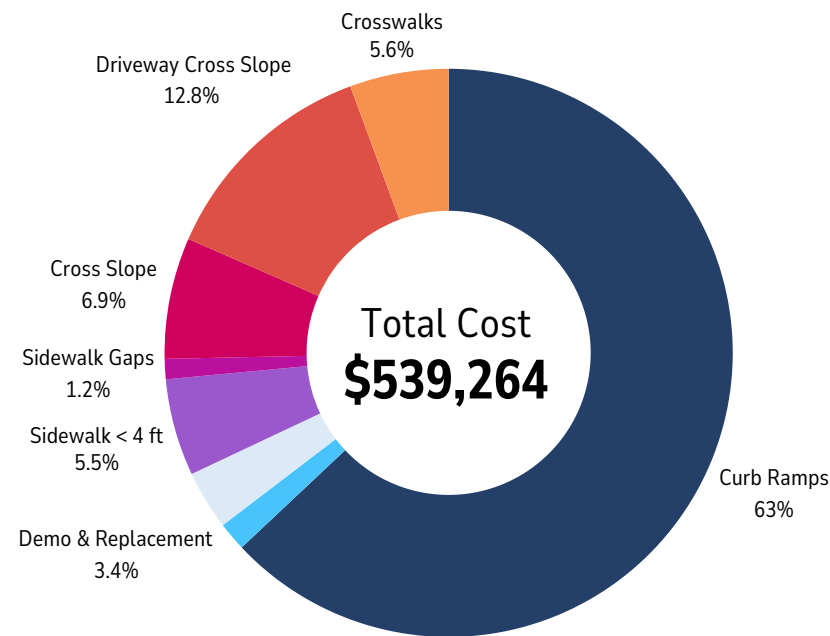
**25% of the Sidewalk Demolish & Replacement** is completed during the 5th year. The total cost is **\$23,915 for 59 locations**.

**4.6%**  
OF ENTIRE PLAN

Canton's Est. Budget will cover 4.6% of remediation in the fifth year.



# LONG-TERM COSTS



## Long-Term Management Plan

**Total Remaining Cost** estimated for repairs is **\$539,264** to cover 36% of remaining repairs.

**36%**  
OF ENTIRE PLAN

CURB RAMPS	Cost (\$)	Locations
P1 - Curb Ramps	\$0	0
P2 - Curb Ramps	\$261,401	69
P3 - Curb Ramps	\$15,154	4
P4- Curb Ramps	\$56,828	15

OBSTRUCTIONS		
Vegetative	\$0	0
Ground	\$5,412	10
Vertical	\$3,247	6

SIDEWALK CONDITIONS		
Demolish & Replace	\$17,809	44
Vertical Height Displacements	\$0	0
Sidewalk < 4 ft.	\$29,226	72
Sidewalk Gaps	\$6,089	15
Cross Slope	\$36,532	90
Driveway Cross Slope	\$67,787	167
Crosswalks	\$29,779	21

	Current Condition		Year 1			Year 2			Year 3			Year 4			Year 5			Longterm		
	Locations/ Panel Number	Total Cost	Locations	Remediation Budget	% Remediated	Locations	Budget	%	Locations	Budget	%	Locations	Budget	%	Locations	Budget	%	Locations	Cost Remaining	% of Locations Remaining
<b>Curb Ramp</b>																				
P1 - Curb Ramp	9	\$ 31,500	3	\$ 10,500	33%	3	\$ 10,710	33%	3	\$ 10,924	33%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%
P2 - Curb Ramp	78	\$ 273,000	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	3	\$ 11,145	4%	6	\$ 22,735	4%	69	\$ 261,401	88%
P3 - Curb Ramp	4	\$ 14,000	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	4	\$ 15,154	100%
P4 - Curb Ramp	15	\$ 52,500	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	15	\$ 56,828	100%
<b>Obstructions</b>																				
Vegetative	42	\$ 6,300	15	\$ 2,250	36%	15	\$ 2,295	36%	12	\$ 1,873	29%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%
Ground	10	\$ 5,000	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	10	\$ 5,412	100%
Vertical	6	\$ 3,000	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	6	\$ 3,247	100%
<b>Sidewalk Conditions</b>																				
Sidewalk D&R	239	\$ 89,625	20	\$ 7,500	8%	18	\$ 6,885	8%	18	\$ 7,023	8%	80	\$ 31,920	33%	59	\$ 23,915	25%	44	\$ 17,809	18%
Vertical Height																				
Displacements	685	\$ 81,750	211	\$ 26,400	31%	210	\$ 26,760	31%	206	\$ 26,830	30%	27	\$ 3,585	4%	0	\$ -	0%	0	\$ -	0%
Sidewalks < 4 ft.	72	\$ 27,000	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	72	\$ 29,226	100%
Sidewalk Gaps	15	\$ 5,625	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	15	\$ 6,089	100%
Cross Slope > 100 ft.	90	\$ 33,750	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	90	\$ 36,532	100%
Driveway Cross Slope	167	\$ 62,625	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	167	\$ 67,787	100%
Crosswalks	21	\$ 36,750	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	0	\$ -	0%	21	\$ 39,779	100%
<b>Total</b>	<b>1,453</b>	<b>\$ 722,425</b>	<b>249</b>	<b>\$ 46,650</b>	<b>17.1%</b>	<b>246</b>	<b>\$ 46,650</b>	<b>16.9%</b>	<b>239</b>	<b>\$ 46,650</b>	<b>16.4%</b>	<b>110</b>	<b>\$ 46,650</b>	<b>7.6%</b>	<b>65</b>	<b>\$ 46,650</b>	<b>4.5%</b>	<b>513</b>	<b>\$ 539,264</b>	<b>35%</b>

## Data Normalization Process

In our asset management plans, we normalize data to ensure an accurate representation of barrier frequency, particularly when assessing demolition and replacement needs. When collecting data, we document specific instances of barriers and their associated features. For example, we may identify a stretch of sidewalk spanning hundreds of feet that requires demolition and replacement. However, raw length data alone does not accurately reflect the number of sidewalk panels affected or the overall impact of these barriers.

To normalize this data, we convert the total linear footage of a barrier into sidewalk panel-based units. We begin by determining the average sidewalk panel size in the given location, as panel dimensions can vary by municipality or construction standards. Once the average panel size is established, we divide the total length of the barrier by this value. This process allows us to quantify the number of affected sidewalk panels, providing a more precise measure of risk and remediation scope.

By normalizing data in this manner, we enhance the accuracy of our risk assessments and create more actionable asset management plans. This methodology ensures that our clients can prioritize remediation efforts effectively, improving accessibility while optimizing budget allocation. Items normalized in the asset management plan include:

- Sidewalk D&R
- Sidewalk < 4 ft.
- Sidewalk Gaps
- Cross Slope > 100 ft.
- Driveway Cross Slope
- Crosswalks

The numbers in the asset management plan will not match the numbers exactly in the report due to this normalization process.



# APPENDIX B

## Full Street Rankings



**PIM** | PRECISION  
INFRASTRUCTURE  
MANAGEMENT

## Appendix B - Full Street Rankings

Rank	Streets	Len mi	Barriers	Risk
1	THE GREEN	0.07	27	411.29
2	SOUTH ST NO 1	0.14	27	194.58
3	MAIN ST	0.29	49	168.55
4	SPRING ST	0.14	21	145.17
5	CENTER ST	0.23	32	140.25
6	BRIDGE ST	0.62	79	127.19
7	RIVER ST	0.13	15	118.51
8	CANTON GREEN RD	0.05	5	110.13
9	FRONT ST	0.06	6	92.36
10	DYER AV	1.11	101	91.21
11	COMMERCE DR	0.54	36	66.12
12	MAPLE AVE	1.71	83	48.56
13	NORTH ST	0.18	8	44.52
14	SIMONDS AV	0.71	30	42.18
15	DUNHAM RD	0.10	4	41.73
16	ALBANY TPKE	5.47	211	38.57
17	MARKET ST	0.06	2	36.26
18	CHURCH ST	0.11	4	35.29
19	OLD RIVER RD	0.11	2	19.04
20	EAST HILL RD	3.09	24	7.77
21	RIVER RD	8.23	10	1.22



# CANTON, CT GRIEVANCE PROCEDURE AND FORM



**PIM**

PRECISION  
INFRASTRUCTURE  
MANAGEMENT



# Town of Canton Grievance Procedure

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 ("ADA"). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the **Town of Canton**. The Town of Canton Personnel Policy governs employment-related complaints of disability discrimination.

## Submission Instructions

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or his/her designee as soon as possible but no later than 60 calendar days after the alleged violation to:

**Name:** Neil Pade

**Title:** ADA Coordinator

**Phone:** 860-693-7856

**Address:** Canton Town Hall, P.O. Box 168, Collinsville, CT 06022

## Investigation Process

Within 15 calendar days after receipt of the complaint, **Neil Pade** or *his* designee will meet with the complainant to discuss the complaint and the possible resolutions. Within 15 calendar days of the meeting, **Neil Pade** or *his* designee will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The response will explain the position of the **Town of Canton** and offer options for substantive resolution of the complaint.

## Appeals Process

If the response by **Neil Pade** or *his* designee does not satisfactorily resolve the issue, the complainant and/or her designee may appeal the decision within 15 calendar days after receipt of the response to the **First Selectman** or *his* designee.





Within 15 calendar days after receipt of the appeal, the **First Selectman** or *his* designee will meet with the complainant to discuss the complaint and possible resolutions. Within 15 calendar days after the meeting, the **First Selectman** or *his* designee will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by **Neil Pade** or *his* designee, appeals to the **First Selectman** or *his* designee, and responses from these two offices will be retained by the **Town of Canton** for at least three years.



## GRIEVANCE FORM

Grievant Name: \_\_\_\_\_

Home Address: \_\_\_\_\_

City, State, and Zip Code: \_\_\_\_\_

Home Telephone: \_\_\_\_\_ Business/Cell Phone: \_\_\_\_\_

Email Address: \_\_\_\_\_

.....  
*This section to be completed only if the aggrieved person is not the individual completing this form.*

Reporting Individual: \_\_\_\_\_

Person(s) Affected by the Situation (if other than reporting individual): \_\_\_\_\_

Address: \_\_\_\_\_

City, State, and Zip Code: \_\_\_\_\_

Preferred Telephone or Email: \_\_\_\_\_

.....  
Program/Activity/Facility Alleged to Be Inaccessible: \_\_\_\_\_

When did the situation occur? (date and time): \_\_\_\_\_





Describe the situation or way in which the program is not accessible, providing the name(s) where possible of the individuals who were involved in the situation (please attach additional pages as needed).

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Have efforts been made to resolve this complaint through the Request for Accommodation with the ADA Coordinator? YES\_\_\_\_\_ NO\_\_\_\_\_

If yes, what were the results? \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Signature:

Date:\_\_\_\_\_

**Send or deliver to:**

Name: Neil Pade

Title: ADA Coordinator

Phone: 860-693-7856

Address: Canton Town Hall, P.O. Box 168, Collinsville, CT 06022

Upon request, reasonable accommodation will be provided in completing this form or copies of the form. Please contact the ADA Compliance Coordinator.

# CANTON, CT COMMUNITY ENGAGEMENT SURVEY



**PIM**

PRECISION  
INFRASTRUCTURE  
MANAGEMENT



## Introduction

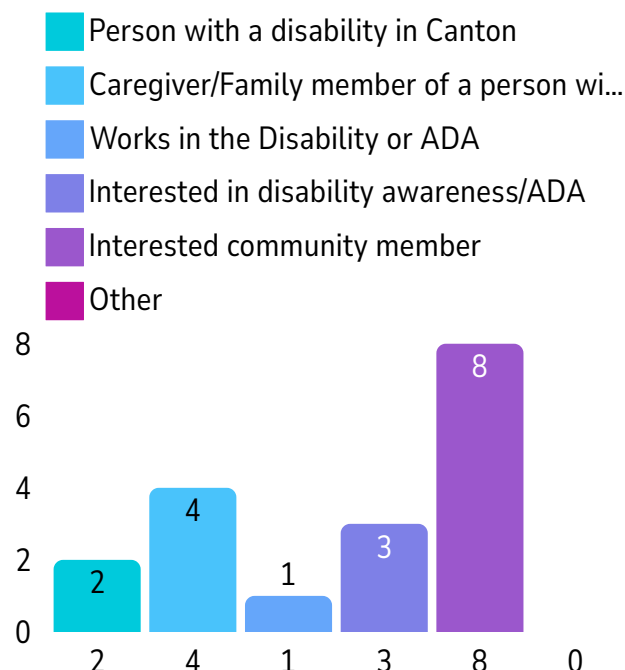
In 2024, the Town of Canton initiated an assessment for its Americans with Disabilities Act (ADA) Transition Plan to improve accessibility across town facilities, programs, and services. A key part of this effort was the ADA Transition Plan Community Engagement Survey, which aimed to gather feedback from residents on barriers faced by individuals with disabilities. The survey covered public rights-of-way, town facilities, parks, programs, and digital accessibility to ensure a comprehensive assessment of accessibility needs.

To further involve the community, the Town of Canton held public engagement sessions, allowing residents to voice concerns and provide suggestions for accessibility improvements. The insights gained from the survey and these discussions will help identify barriers, guide self-evaluation efforts, and shape a detailed transition plan. This plan will outline specific actions and timelines to address accessibility challenges, reinforcing Canton's commitment to fostering an inclusive and accessible environment for all residents.

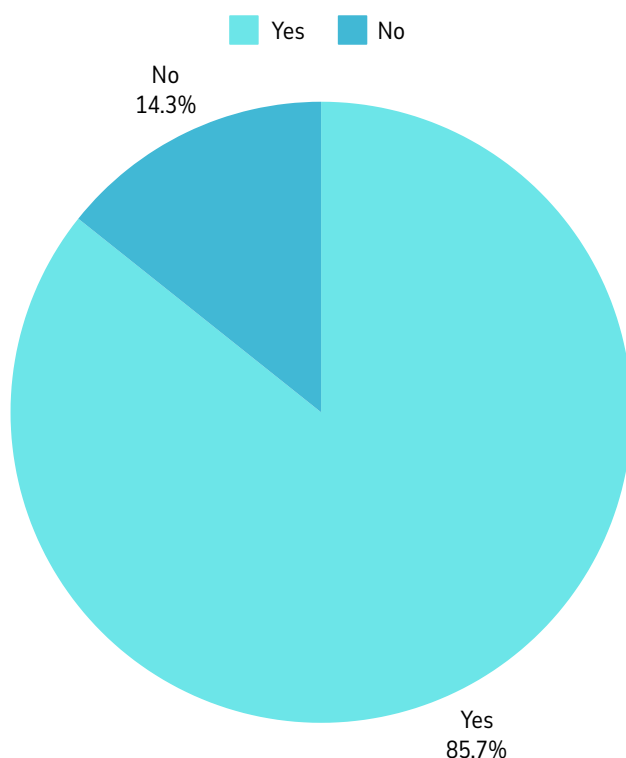


## Interest and Participation

The majority of respondents (57.14%) identified themselves as interested community members, demonstrating strong public engagement with the Town of Canton's ADA Self-Evaluation and Transition Plan. Additionally, 28.57% of respondents selected "Caregiver," with other responses including individuals who work in the Town of Canton, family members of someone with a disability, town planners, and town employees, highlighting a diverse range of stakeholders invested in accessibility and ADA-related initiatives.



## Do you participate in a program or service offered by Indian Trail?



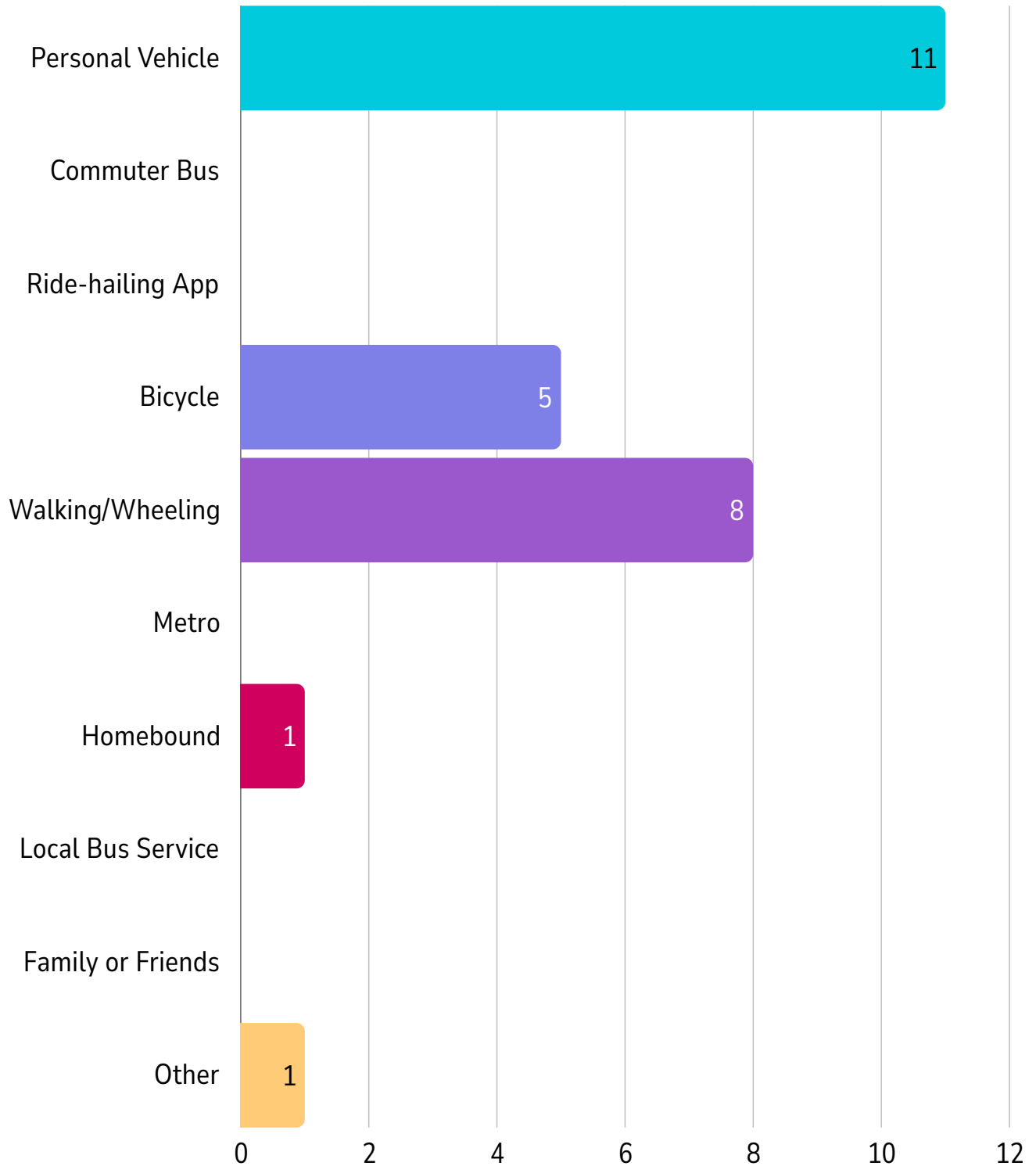
## Reported Programs, Services, and Activities

- **Libraries, Facilities & Trails:** Regular use for leisure, programs, and events.
- **Recreation & Events:** Participation in park activities, town events, and community programs.
- **Community Involvement:** Volunteering, wellness initiatives, and local advocacy.



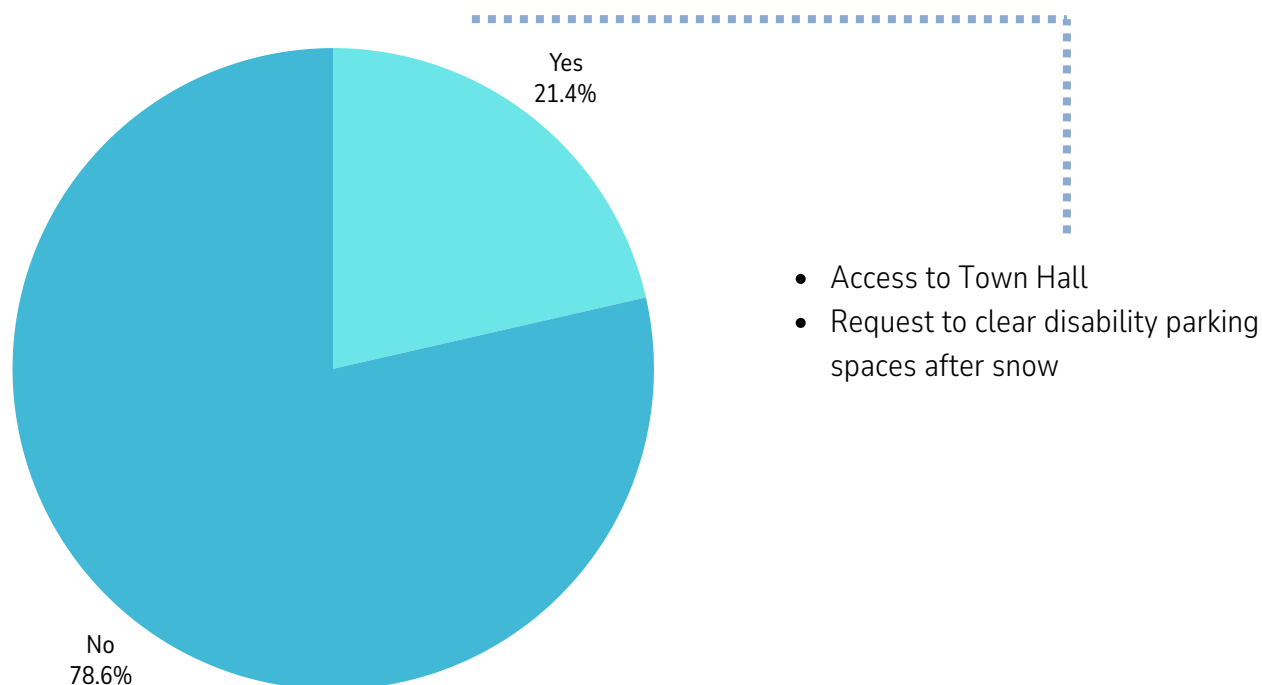
## Transportation

The majority of respondents (78.57%) rely on personal vehicles as their primary mode of transportation, while 57.14% also engage in walking or wheeling. A smaller portion (35.71%) use bicycles, while 7.14% are home-based and 7.14% answered “Other”.



## Assistance

### Have you ever needed help from the Town of Canton?



### Are there any Town services or buildings that are hard to access or could be made better for accessibility? Please list them and share details.

- As long as the sidewalks, entrances are cleared properly they are fine. Cherry Brook Primary needs additional handicapped parking spots in the front of building.
- Stepping up to the sidewalk to enter the rear of the Canton Library - it is quite steep as well as unsafe because of a rounded edge that my feet have almost slipped on before.
- Town Hall.
- I don't physical limitations so right now there are no access issues related to public buildings.
- Town Hall! Voting is at school but problematic when transportation is via handicap van and minimal spots available.
- Ladies rooms can be really tight for wheelchair users.
- No.



## Town Policies

Can you think of any Town policies that are accessible for people with disabilities?

 <div>ALL</div> <div>No additional context is provided.</div>	 <div>WEBSITES</div> <div>Some pages are really confusing. The tax assistance page infers virtually no navigation help and is in a lot of legalese.</div>
 <div>RIGHT OF WAY AND PED TRAFFIC</div> <div><ul style="list-style-type: none"><li>• The ordinance for ensuring snow is removed from sidewalks needs to be enforced. There is one area on Dowd that I have never seen done in 5 years. One time I saw a person in a wheelchair have to go out on to Dowd to get around this section.</li><li>• Kudos to Public Works for getting out on the Farmington River Trail quickly after storms to clear debris.</li></ul></div>	 <div>NONE</div> <div>Three respondents did not understand the question or had no further comments.</div>

## Canton Staff

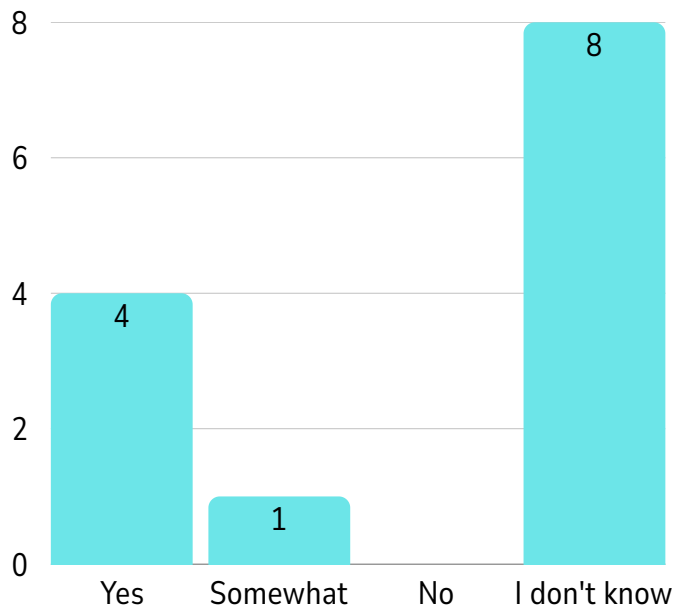
**Do you know which Town staff member to contact if you need help accessing a service or building?**

Common Answers:

- Kara at Senior Center.
- CAO at Town Hall.
- Librarians at Town Hall.



**Do Town staff usually help when solving problems for people with disabilities? Are they supportive and positive?**



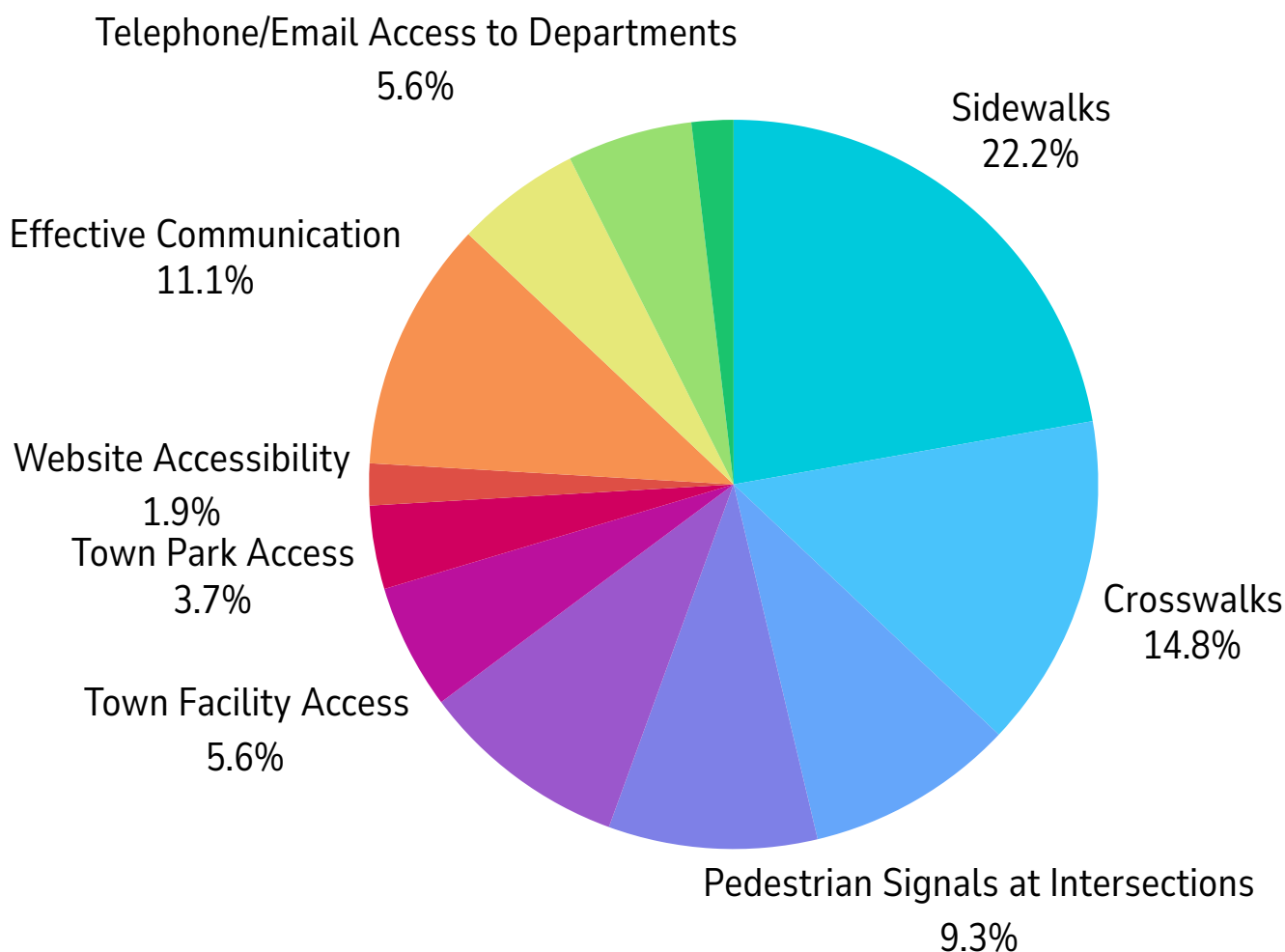
4 (30.77%) stated that town staff are helpful in addressing accessibility issues, while 1 (7.69%) found them somewhat helpful. No respondents (0%) reported negative experiences, and 8 (61.54%) selected "Do not know"



## Highest Priorities to Improve Accessibility

### What are the top priorities for Canton to improve accessibility?

Sidewalk improvements (78.35%), crosswalk enhancements (55.67%), and pedestrian signals at intersections (49.48%) were the top concerns. Respondents also highlighted the need for more curb ramps (43.30%), better accessible parking at town facilities (22.68%), and improved park access (22.68%).



## General Concerns

### **Do you have any more feedback about accessibility in Town parks? Is there anything else you'd like to share about public facilities?**

- More accessible trails and playground equipment. More wheelchair ramps.
- Lack of walkways in town parks.
- The rail trails are a huge town amenity and I often see people with disabilities or seniors using them. But the section in Burlington is now so rough and bumpy, it's blocking access for people in Canton. It needs repaving.
- Our town park is below standard when compared to virtually every other town park. So much money has been allocated to park upgrades. The money was poorly spent. The location of adult fitness equipment next to the children's play area is concerning.

### **Do you know of any sidewalks in Canton that are hard to use or unsafe? If yes, please share the general address or cross streets. You can also give other location details and explain why the sidewalk is a problem.**

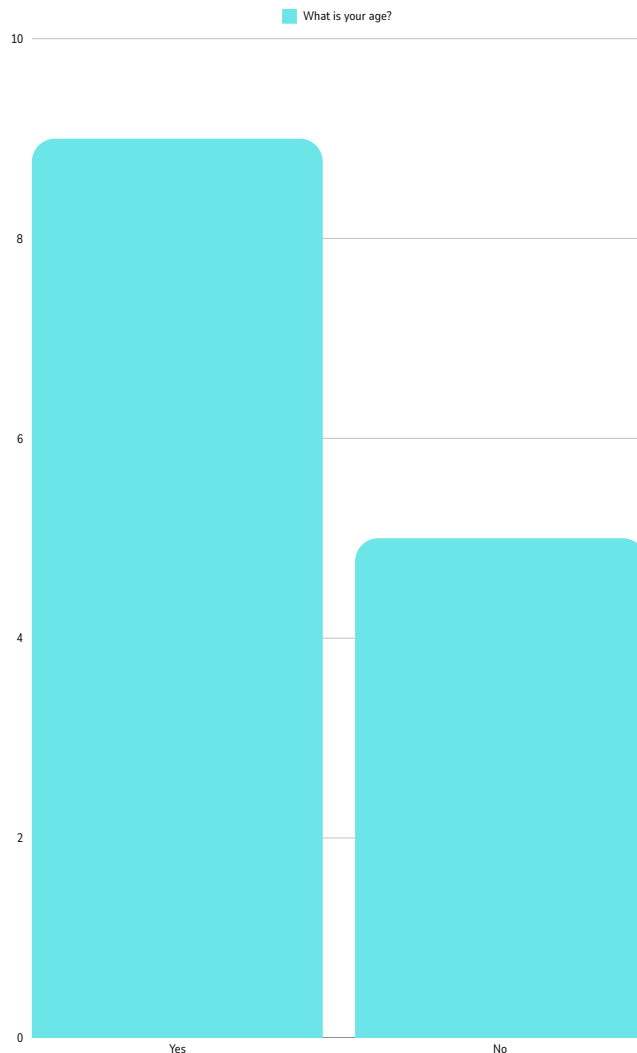
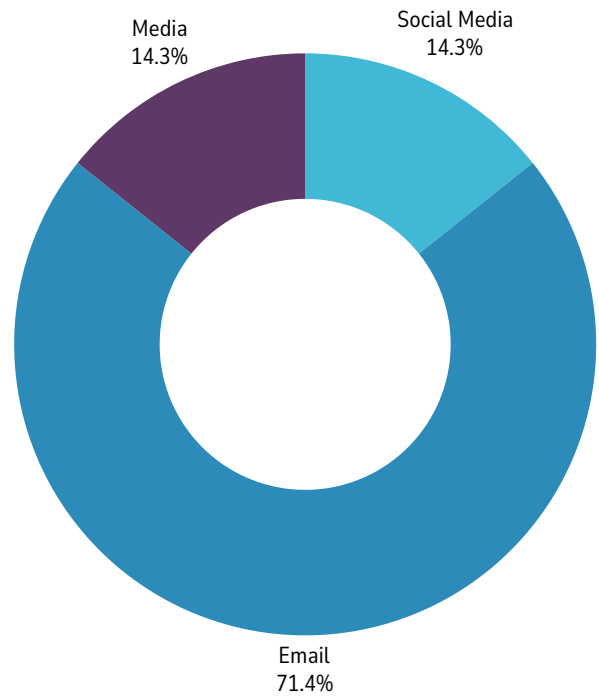
- The sidewalks along RT 44. When I have had to wheel a suitcase along there to catch the bus. Very difficult plus actually broke the wheel on it. Wheelchairs are sturdier but it makes me wonder about the wear and tear on them.
- A sidewalk on Lawton Road would be great!
- The sidewalk running parallel and next to ShopRite of Canton is too high to step up or down from along the drive lane.
- No side walks on the way to memorial century and bushes are overgrown forcing you to walk in the road.
- No sidewalks on River road.
- Make access to public buildings easier from throughout town.
- The bottom of Center Street crossing rte 179 is extremely steep and difficult to negotiate without disabilities.
- Not sure what you could do, but connecting the rail trail to the Shoppes is too steep for many.
- None.



## Respondent Information

### How did you hear about this community engagement survey?

Respondents learned about the survey's availability primarily from email (71.4%), while traditional media and social media each made up 14.3% of total responses.



### Can the Town or its representatives contact you for more information about any of your answers? Do they have your permission to follow up?

The majority of respondents (64.29%) gave permission for Town officials to contact them about their responses.